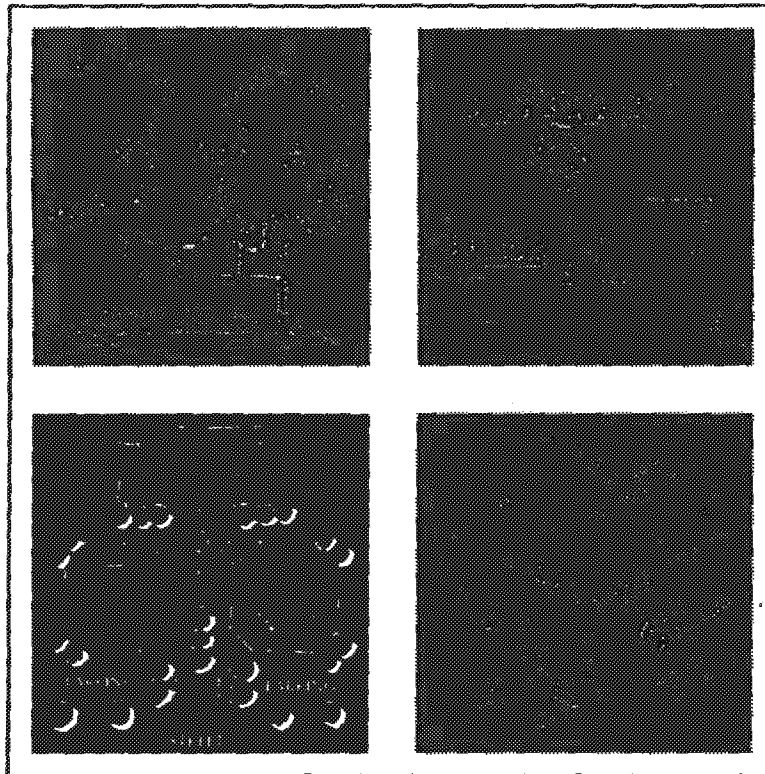


INGLEWOOD INTERNATIONAL BUSINESS PARK

S P E C I F I C P L A N



Prepared by:



ENVICOM
CORPORATION

In association with:

Economics Research Associates
Terry A. Hayes Associates

Adopted:

December 21, 1993

Inglewood International Business Park Specific Plan

Prepared For:

**City of Inglewood
One Manchester Boulevard
Inglewood, California 90301
Contact: William Barnett**

Prepared By:

**Envicom Corporation
28328 Agoura Road
Agoura Hills, CA 91301
Contact: Lisa A. Brownfield, AICP**

in association with

**Economics Research Associates
10990 Wilshire Boulevard, 16th Floor
Los Angeles, California 90024**

and

**Terry A. Hayes Associates
100 Corporate Point, #105
Culver City, California 90230**

**Adopted
December 21, 1993**

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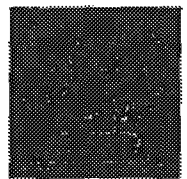
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Introduction



INGLEWOOD INTERNATIONAL
BUSINESS PARK

The Inglewood International Business Park Specific Plan is a document establishing land use, infrastructure requirements and the design character for the area south of Century Boulevard, and east of Prairie Avenue in the City of Inglewood. Future decisions by the City regarding land use, transportation, architectural design, and location of infrastructure are to be guided by the Plan. The Specific Plan serves to clarify the City's intentions with respect to the rights and expectations of the general public, property owners, and the greater business community. Through the Plan, the City can inform these groups of its goals, policies, and development standards.

The Inglewood International Business Park (IIBP) Specific Plan has been prepared in accordance with California Government Code Sections 65450 through 65457 governing the preparation and contents of specific plans.

These sections state that a specific plan shall include text and a diagram specifying, in detail, the following:

- location and extent of land uses;
- description and location of public and private transportation, sewerage, water, drainage, solid waste disposal, energy and other essential infrastructure;
- development criteria and standards;
- program implementation measures; and
- the relationship between the specific plan and the general plan.

STRUCTURE OF THE DOCUMENT

This document is divided into three sections. The first section is this introduction describing the specific plan process and the specific plan's relationship to other plans. The second section constitutes the Plan and its implementation. The third section, the Appendix, presents background land use and environmental data and analyses. In addition, a separate Environmental Impact Report (EIR), documenting the area's existing conditions and the Plan's impact on the existing conditions, has been prepared.

The second section, the Plan, contains a statement of goals and policy regarding the Specific Plan and a series of standards for the following topics: land use, urban design, circulation, site access and parking, public works and public services, and the environment. In addition, the Plan prescribes implementation through ordinance.

THE PLANNING PROCESS

The City of Inglewood Community Development and Housing Department directed the specific planning effort. Envicom Corporation, an urban planning and environmental research consulting firm, prepared the Plan in conjunction with Economics Research Associates and Terry A. Hayes Associates. The IIBP team worked with locally elected and appointed City officials, City staff, owners of property in the Specific Plan area, and interested citizens through a series of meetings and public workshop. The workshop which addressed land use policies was held on February 6, 1992.

RELATIONSHIP TO OTHER PLANS

The City of Inglewood General Plan is comprised of eight elements; one of the elements, the Land Use Element, addresses the subject area as requiring further planning, redevelopment and revitalization. The Land Use Element specifically states that the area bounded by Crenshaw Boulevard, La Cienega Boulevard, Century Boulevard, and 104th Street be designated as "Industrial" and developed with industrial and commercial office park uses. Uses to be included in the industrial and office parks are light manufacturing, assembly, office, and wholesale/warehousing. Furthermore, the Land Use Element's objectives include:

A. Relationship with the General Plan

- promoting sound economic development, increasing employment opportunities by responding to economic change;
- fostering revitalization or recycling of residential areas which cannot provide decent living environments because of jet noise;
- improving visual appearances and economic conditions of existing arterial development; and
- providing a diverse industrial base, continuing to improve industrial areas by increasing and improving infrastructure and eliminating incompatible or blighted uses.

The Specific Plan is anticipated to fulfill these objectives through:

- the anticipated removal of area's residential units that are impacted by noise attributable to the aircraft operations of Los Angeles International Airport, and the relocation of the residents;
- the provision of appropriately sized industrial designated parcels;
- the provision of vehicular and pedestrian circulation facilities, sanitation, sewer facilities, water, storm drain facilities, utilities and other adequately sized infrastructure that support the projected industrial park use; and
- the provision of the uses in an aesthetically pleasing "campus like" setting.

Therefore, the Specific Plan will establish use, procedures, and the implementation programs which put into action the objectives specified in the General Plan.

B. Relationship with the Interim Development Ordinance

In July 1989, the Inglewood City Council instituted a moratorium to forestall further development in the area until a specific plan could be prepared. In a second action, Ordinance 90-30, the City Council amended the moratorium to establish an Interim Development Regulation.

The Interim Development Regulations permit development applications within the area if the application meets specific criteria. The criteria includes required right-of-way dedication, specified parcel size (over 70,000 square feet if application received prior to March 1, 1991 and over 100,000 square feet if application received after March 1, 1991), 102nd Street or appendage road access, on-site parking location standards, building design criteria and setbacks, and infrastructure construction requirements. As of May 1991, one project has been submitted to the City for review and approval. This project, approximately 70,000 gross square feet of industrial use, is currently undergoing City staff review. The project is located on the southern side of 102nd Street midblock between Doty and Yukon Avenues.

DESCRIPTION OF THE INGLEWOOD INTERNATIONAL BUSINESS PARK

Regionally, the area is currently accessed from the north-south Interstate Route 405 (San Diego Freeway) and more distantly from the north-south Interstate Route 110 (Harbor Freeway) as shown in Figure 1. Interstate Route 105, the east-west Century Freeway, is anticipated to be completed in 1993. In all cases, vehicular traffic would be required to use major arterials such as Century Boulevard and Prairie Avenue to access the IIBP site.

The IIBP is located in the southern portion of the City of Inglewood. The area boundaries are 102nd Street to the north, Yukon Avenue to the east, 104th Street to the south, and Prairie Avenue to the west. The area is bisected by the north-south running Doty Avenue (Figure 2). The area is flat, previously graded land, and approximately 44 acres.

Approximate travel distances from significant locations in the greater Los Angeles metropolitan area to the IIBP are:

<u>Location</u>	<u>Approximate Travel Distance</u>
Los Angeles International Airport	3.0 miles
Downtown Los Angeles	10.0 miles
Port of Los Angeles	15.0 miles
San Diego Freeway	2.5 miles
Harbor Freeway	3.0 miles
Proposed Century Freeway	0.75 mile

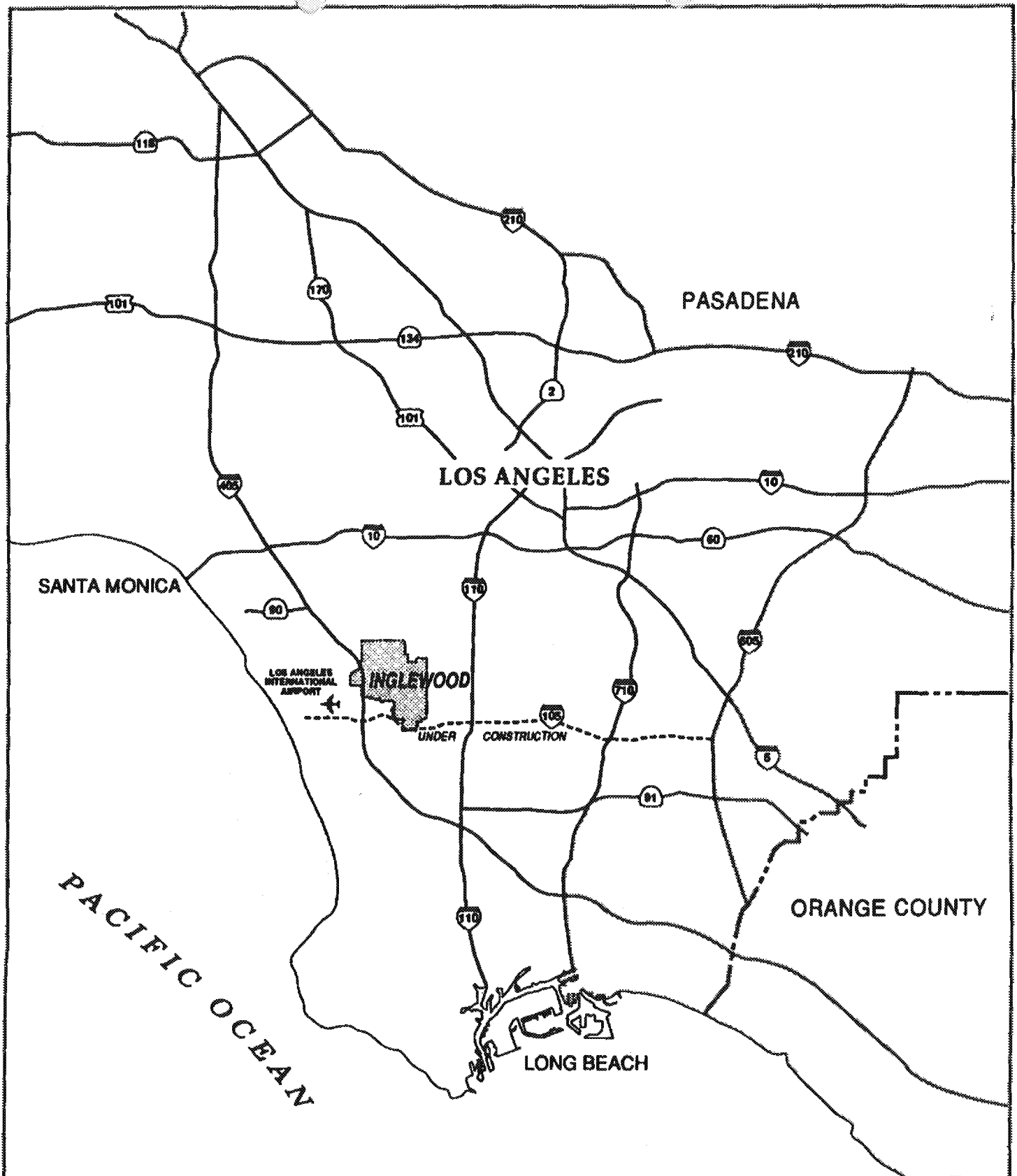


FIGURE 1

**REGIONAL LOCATION
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan**

0 Feet 50 100 150



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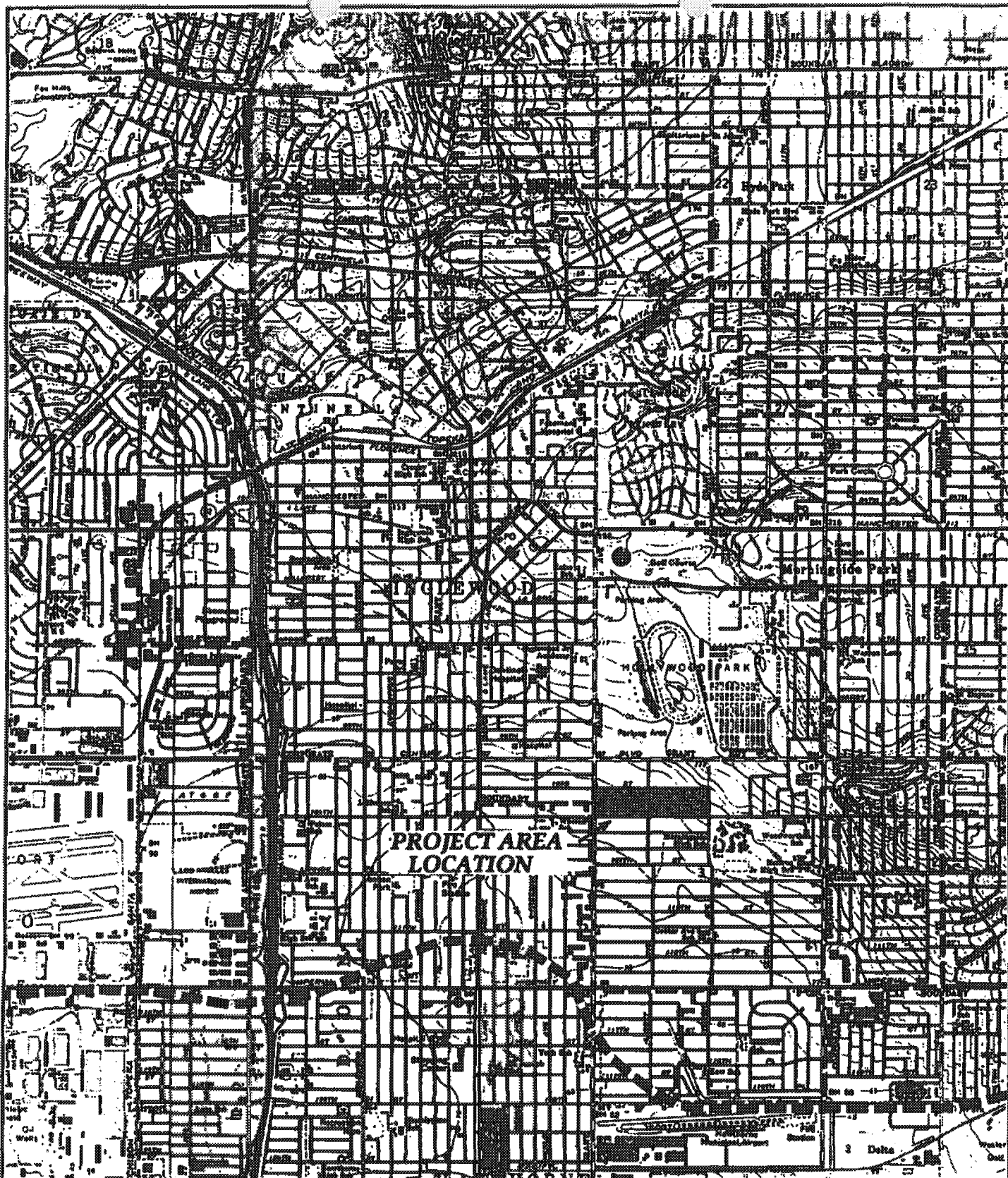
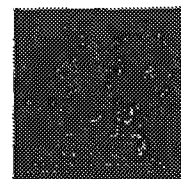


FIG. 2 LOCAL VICINITY

**INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan**



The Plan



INGLEWOOD INTERNATIONAL
BUSINESS PARK

The Specific Plan's goals and general objectives function as the framework or the cornerstone for the Inglewood International Business Park Specific Plan. The goals and the general objectives provide the basis for the Specific Plan's development standards. The Specific Plan establishes development standards for land use, urban design, circulation, site access, public works, public services, noise, and air quality. The development standards are discussed in the subsequent subsections of this Plan.

Goals

The IIBP Specific Plan enables private development to create an aesthetically pleasing business park which facilitates large scale corporate users while benefitting the City of Inglewood and the residents who live in the surrounding neighborhood. To ensure the business park's development, it shall be the goals of the City of Inglewood to:

- 1) create a economically viable and diverse business park that generates employment opportunities for residents and revenue for the City of Inglewood;
- 2) foster the change of existing residential uses that do not provide decent living environments because of overhead jet noise to other noise compatible land uses;
- 3) create a visually attractive business park using streetscape, landscape, and urban design features to unify the individual business structures into a cohesive "campus" like commercial-industrial center;
- 4) create a vehicular circulation system that provides efficient access to the IIBP businesses while protecting nearby residents from automobile and truck related impacts; and
- 5) implement the long established land use policies of the General Plan for the area, comply with the Federal Aviation Administration's (FAA) airport land use compatibility policies, and comply with the State of California airport noise standards.

General Objectives

- 1) Recycle and improve the physical character of the IIBP area to attract large scale corporate users.
- 2) Improve the physical relationship between the IIBP area and the adjacent residential neighborhood.
- 3) Accommodate new development in new structures which comply and enhance the urban design characteristics as provided in the Specific Plan.
- 4) Improve the vehicle access and the parking relationship with the associated business structure above the level that which would have occurred without the Specific Plan's urban design characteristics.
- 5) Ensure adequate provision and maintenance for infrastructure, support services, and circulation improvements to accommodate demands generated by the IIBP.

The purpose of the Land Use development standards is to provide for the continued existence and new development of commercial, industrial, and research and development uses that facilitate large scale corporate users, generate employment and revenue base for the City, while being compatible with adjacent uses.

Standards

LU1 The area adjacent Prairie Avenue and 200 feet east, and bounded by 102nd Street and 104th Street shall be designated as General Commercial (C-2) as shown in Figure 3.

LU2 The area between Yukon Avenue, the General Commercial designated parcels, and fronting 104th Street shall be designated as Limited Manufacturing (M-1L) but it shall also contain a Park Overlay as shown on Figure 4.

The Park Overlay will permit a seven to ten acre active park. Amenities permitted in the park include, but are not limited to:

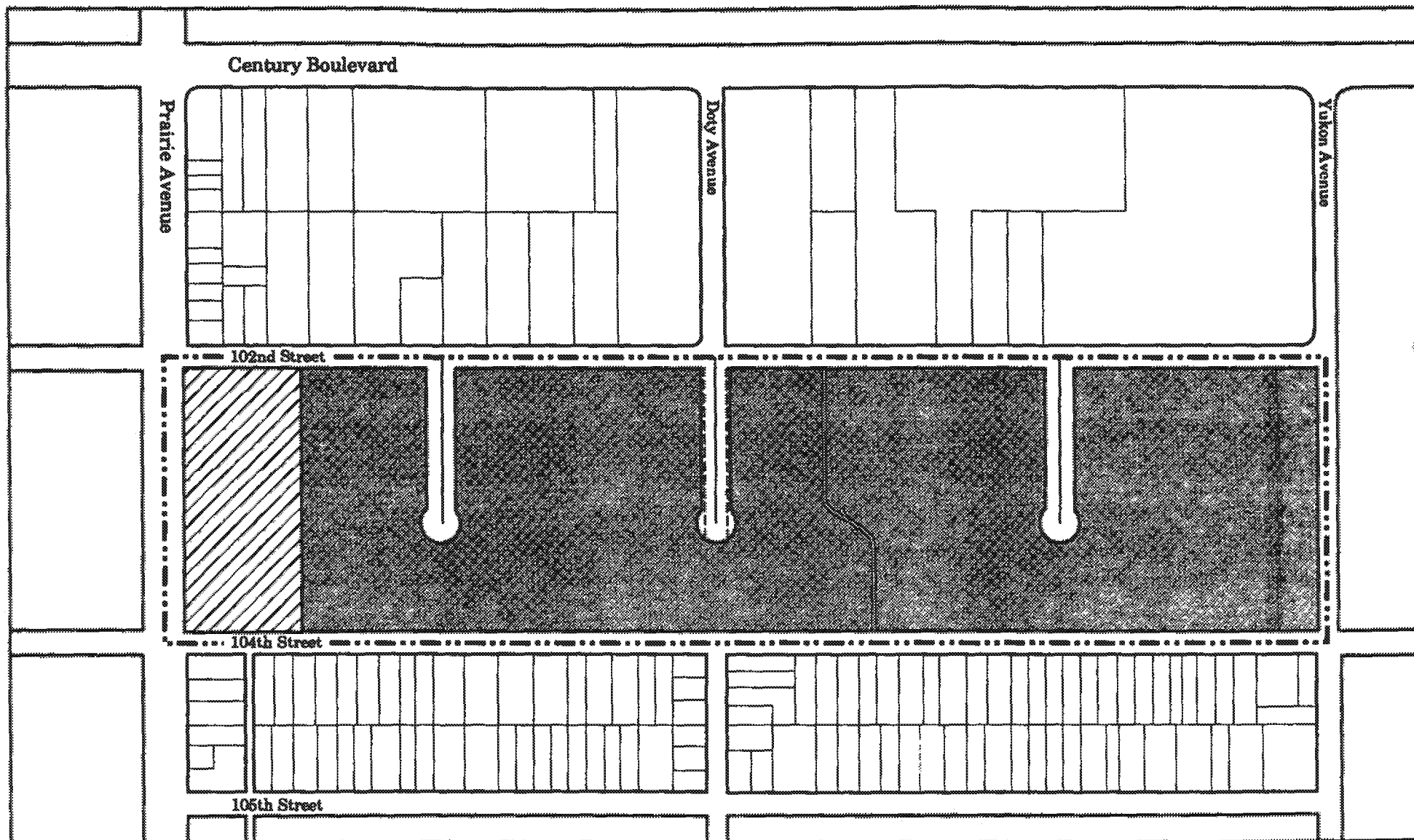
- softball fields, soccer fields;
- swimming pool, diving pool; and
- volleyball courts, basketball courts.

The City of Inglewood shall determine location, size, and uses of the active park.

LU3 The remaining IIBP area shall be designated as Limited Manufacturing (M-1L).

A) Permitted Uses in the M-1L Limited Manufacturing Zone are:

- Any kind of assembly, manufacture or processing of products other than those which may be obnoxious or offensive by reason of emission of noise, odor, dust, smoke, vibration, or similar causes;
- Cabinet shops and furniture manufacture;
- Food packaging and bottling plants;
- Book publishing, printing, and binding;
- Freight terminals and parcel delivery terminals;
- Laboratories, research, testing and medical;
- Storage and warehousing;
- Manufacturers' sales, retail and wholesale, of manufacturers' products exclusively;
- Retail sales of new merchandise only:
 - a. if conducted within a single tenant structure having a minimum gross floor area of 75,000 square feet; or



ZONING DESIGNATIONS **INGLEWOOD INTERNATIONAL** **BUSINESS PARK** **Specific Plan**



C-2 (General Commercial)



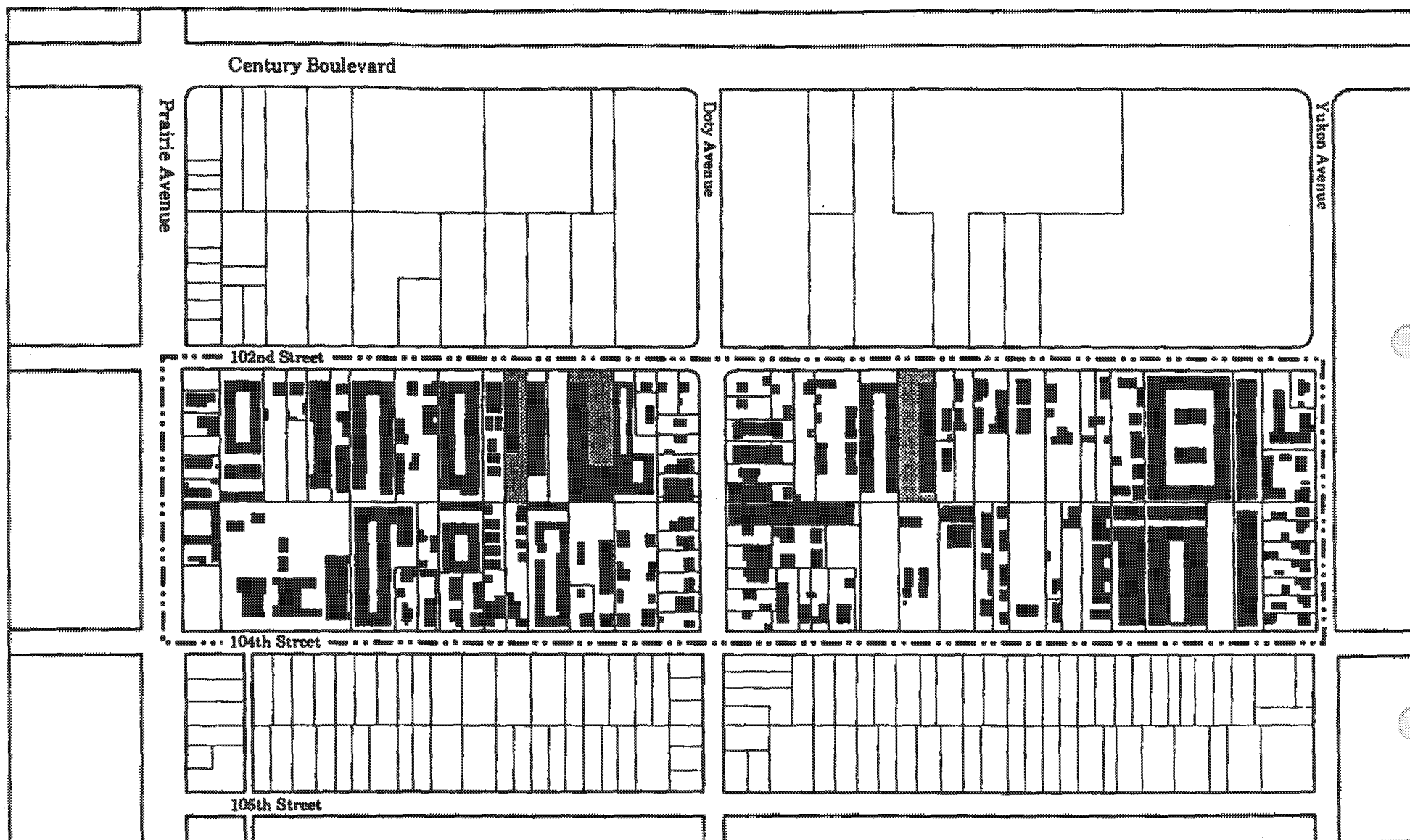
M-1L (Limited Manufacturing)

FIGURE 3



Acres	
0.5	
0.25	

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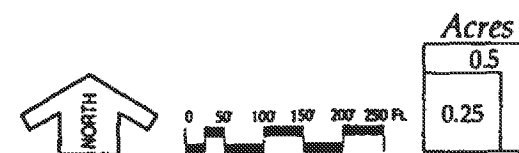
EXISTING STRUCTURES INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan

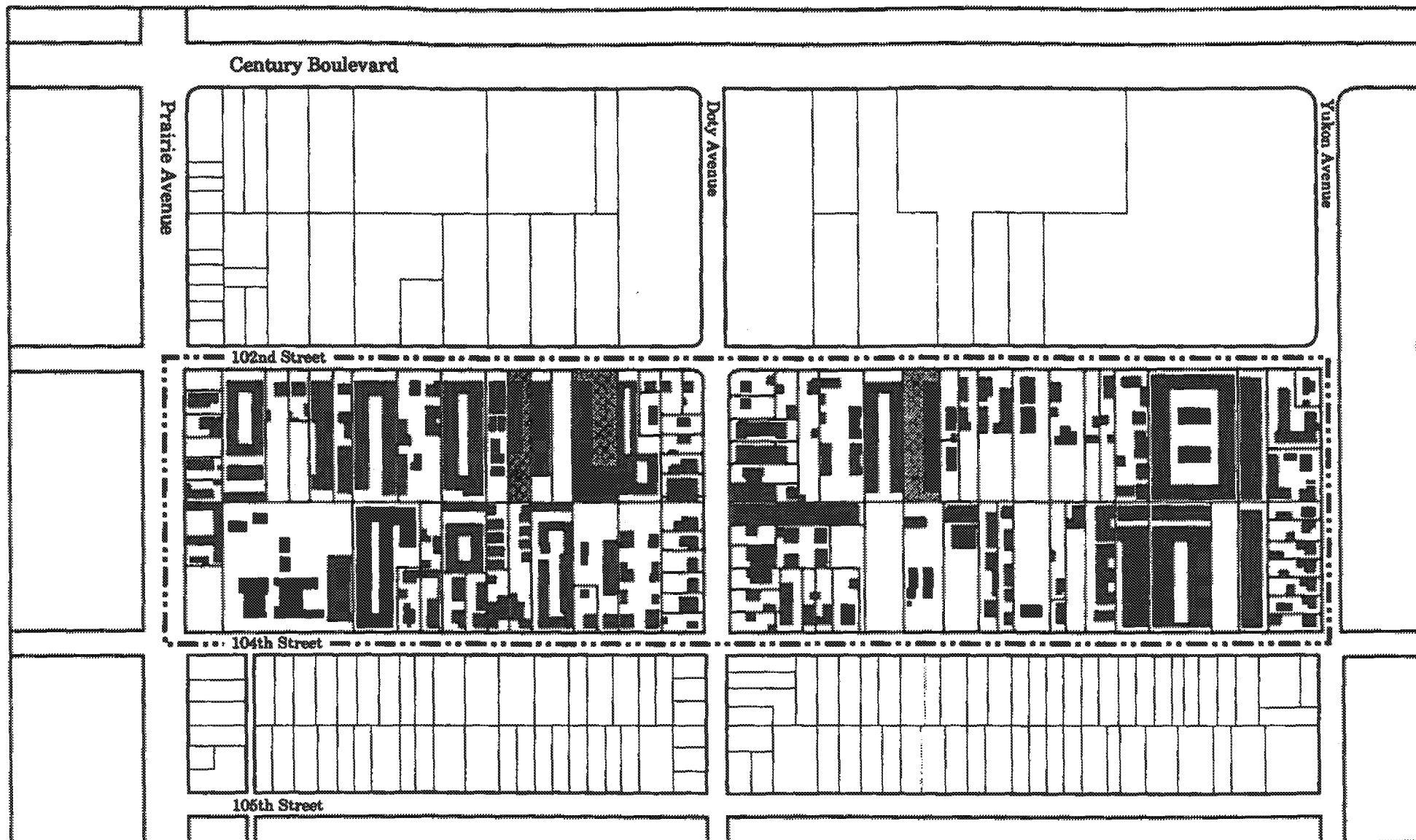


GENERALIZED BUILDING FOOTPRINTS

INDUSTRIAL LOTS

FIGURE 4





EXISTING STRUCTURES INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan



GENERALIZED BUILDING FOOTPRINTS

INDUSTRIAL LOTS

FIGURE 4



Acres	
0.5	
0.25	

ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates

b. if conducted within a shopping center (multiple tenant structure or structures) having a minimum total gross floor area of 100,000 square feet, subject to Planned Assembly Development approval pursuant to Article 18 of Chapter 12 of the Inglewood Municipal Code, and consisting of only those commercial uses and services permitted in the C-2 Zone.

- Professional and medical offices;
- Financial institutions;
- Hotels (inclusive of restaurants and shops intended to primarily serve hotel patrons, with a minimum of 100 guestrooms per facility);
- Employee food facilities and service stores when developed adjunct to and integrated within other permitted uses;
- Utility and public service uses; and
- Daycare for employees' children as an ancillary use.

B) Prohibited Uses in the M-1L Limited Manufacturing Zone are:

- Retail stores, except as described above;
- Vehicle sales;
- Vehicle servicing (except when developed subordinate and adjunct to and integrated within any permitted use);
- Vehicle storage (customer and employee parking, and parked freight vehicles excepted);
- Any outside manufacture, sales and storage of any materials, products, or merchandise;
- Animal rendering and animal kennels and pens;
- Foundries;
- Junk or salvage yards;
- Petroleum and chemical processing and distillation;
- Schools and churches; and
- Trailer camps/trailer parks.

C) Permitted Uses in the C-2 General Commercial Zone are:

- Retail sales of merchandise including but not limited to:
 - a. Apparel, millinery, shoes, jewelry;
 - b. Books, stationery, gifts, toys, cameras, pets, flowers;
 - c. Furniture, appliances, floor coverings;
 - d. Groceries, prepared food, meat, fish;

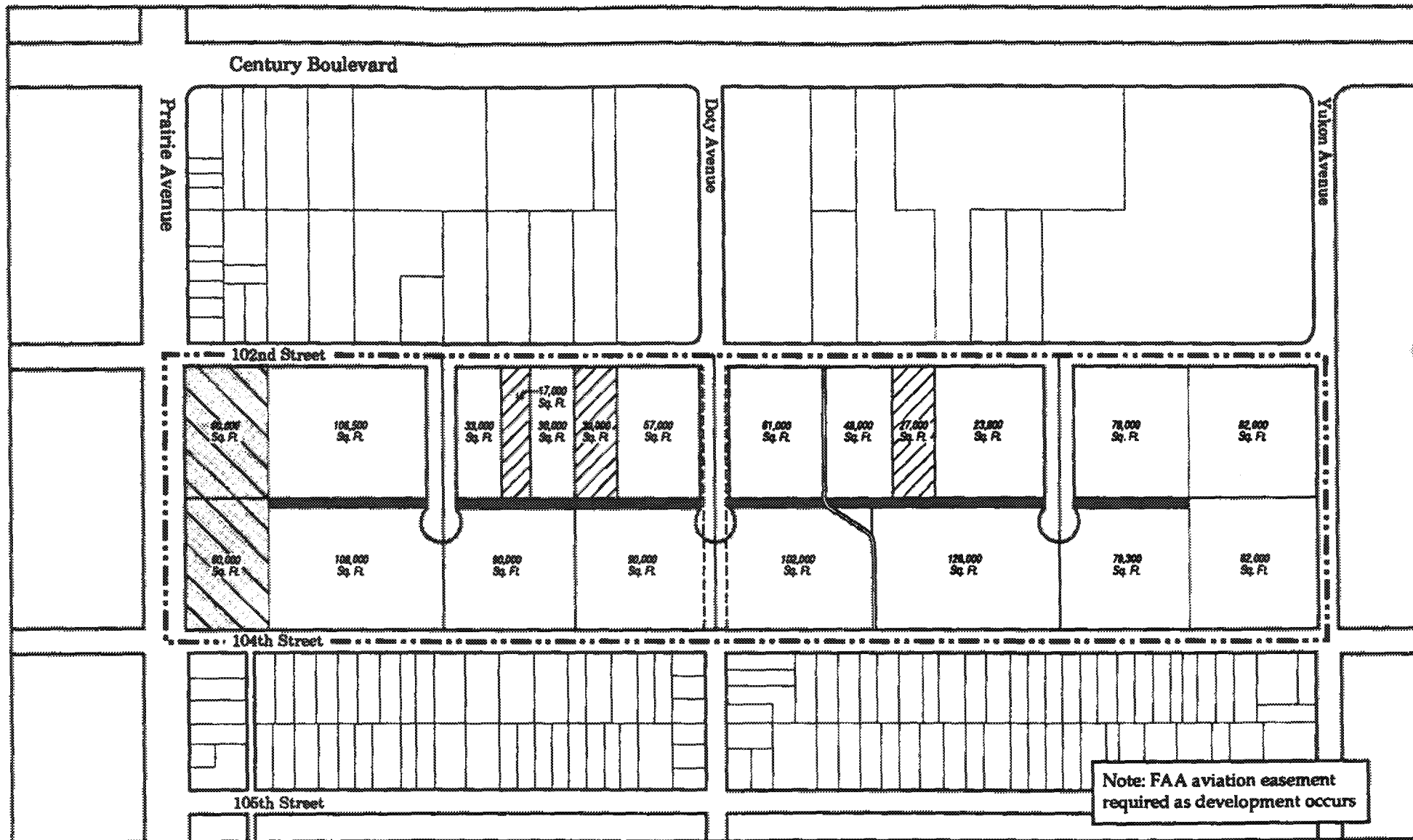
- e. Automotive parts, hardware, paint;
- f. Antique art and antique furniture;
- Financial and insurance institutions;
- Professional, medical offices, laboratories and pharmacies;
- Restaurants, cafeterias, doughnut shops, bakeries (baked good sold only on premises);
- Bars, nightclubs, supper clubs, dance halls, live performance theaters, and the like, subject to Special Use Permit approval. (Bona fide restaurants serving only beer and wine without live entertainment are permitted without Special Use Permit approval.);
- Service shop for watches, keys, shoes, small household appliances, dry cleaning, tailoring, printing, and plumbing;
- Beauty shops and barbershops, including training schools for same if open to the public for patronage;
- Multiple tenant shopping centers, including interior shopping malls, containing two or more of the above businesses;
- Theaters, bowling alleys and billiard or pool halls;
- Hotels or motels, with a minimum of 100 guestrooms per facility;
- Trade or business schools, adult or proprietary schools, colleges or universities, group counseling facilities, and the like, when located on other than street level and subject to Special Use Permit approval;
- Thrift stores, only if operated by non-profit organizations and subject to Special Use Permit approval;
- Public and quasi-public uses;
- Parking lots (vehicle storage prohibited);
- Liquor stores, subject to Special Use Permit approval if located within 300 feet of any residentially zoned property;
- Arcades, game, film or video, subject to Special Use Permit approval; and
- Accessory uses customarily incidental to any of the above uses.

D) Prohibited Uses in the C-2 General Commercial Zone are:

- Automobile sales;
- Rental of trailers for hauling purposes;
- Plant nurseries;
- Newsstands;

- Adult businesses;
- Bath houses, turkish baths, massage parlors, and similar uses; and
- Swap meets or flea markets.

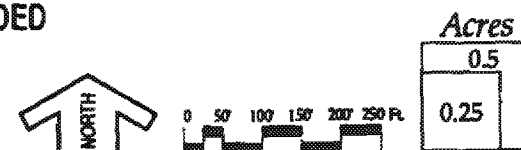
- LU4 Minimum lot sizes shall be established in accordance with Figure 5.
- LU5 Consolidation of lots to create larger lots are permitted in accordance with Figure 6.
- LU6 Maximum development capacity for the entire Specific Plan site shall be 1,105,529 square feet of commercial and/or industrial use. The City of Inglewood shall determine the appropriate distribution of the permitted development capacity.
- LU7 An internal parcel and circulation pattern that facilitates both large lot and small lot development shall be created with the following characteristics:
- A) three cul-de-sacs providing truck and automobile access from 102nd Street to the lots fronting 104th Street as shown in Figure 7;
 - B) lots fronting 104th Street which have direct access to cul-de-sac are required to maintain a 30 feet access easement between the cul-de-sac and any adjacent lot that does not have direct access to 102nd Street, Yukon Avenue, Prairie Avenue or another cul-de-sac as shown in Figure 7;
 - C) lots which receive benefit from the 30 feet access road are required to be responsible for the access road's construction and physical maintenance; and
 - D) those lots with an easement are required to maintain the access road debris free, and useable for truck and automobile travel. Business permits, certificates of occupancy, code enforcement, and other operating permits shall be tied to the satisfactory maintenance of the access road.
- LU8 Commercial uses are required to be compatible with surrounding land uses, including:
- A) limitation on type of use, hours of operation, attendance to control noise, traffic, and lighting impacts;
 - B) location of site access points away from residential uses; and
 - C) inclusion of landscape setbacks, walls, plantings, and other elements to buffer adjacent land uses.
- The City of Inglewood shall determine appropriate compatible uses and their characteristics (hours of operation, attendance limits, noise control, site access points, etc.) during the project's design and site plan review.



SMALL PARCEL CONCEPT INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan

- COMMERCIAL DESIGNINATION
- ACCESS EASEMENTS, IF NEEDED
- EXISTING INDUSTRIAL

FIGURE 5



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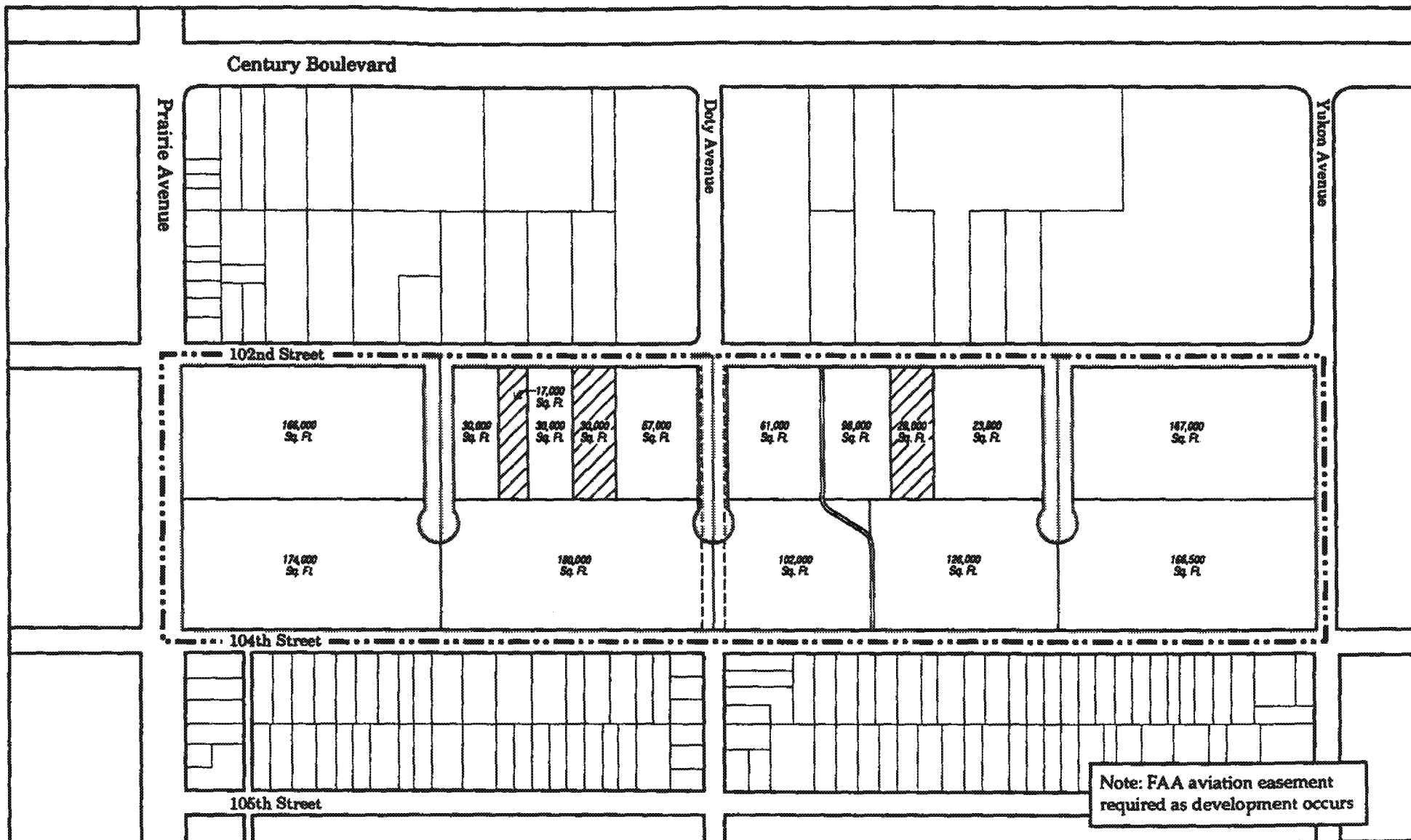
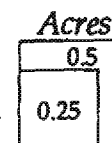


FIGURE 6

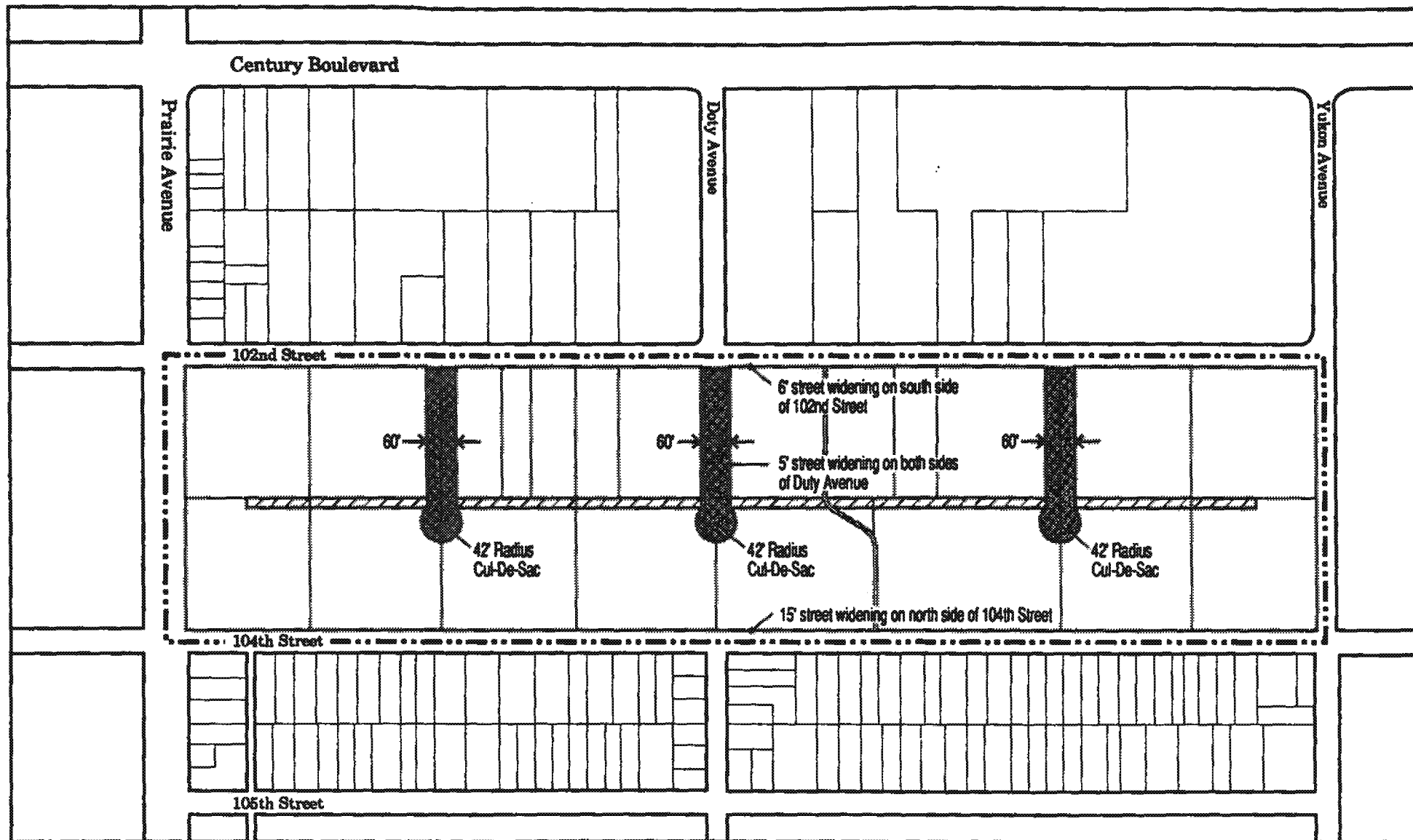
LARGE PARCEL CONCEPT **INGLEWOOD INTERNATIONAL** **BUSINESS PARK** **Specific Plan**



EXISTING INDUSTRIAL



ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates



CIRCULATION SYSTEM INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan



NEW CUL-DE-SAC



POTENTIAL ACCESS EASEMENT



Acres	
0.5	
0.25	

FIGURE 7

- LU9** Entertainment and other uses characterized by high activity levels shall provide adequate safety measures preventing "spill over" impacts on adjacent properties as determined by the City of Inglewood during the project's design and site plan review.

The intent of the Urban Design development standards is to establish the IIBP as a distinctive, private development business park through the siting, massing, height and architectural character of buildings and a system of landscaped spaces and visual elements which provide an organizational framework and visual unity.

Standards

Siting

UD1 Setbacks are to be maintained along all street frontages of the IIBP Specific Plan area. A building setback is the area from the property line to the edge of structure in which no buildings may exist or otherwise be constructed. A parking setback is the area from the property line to the edge of a parking surface in which no surface designed for the parking or stowing of vehicles or materials is permitted as shown in Figures 8 and 9.

IIBP setbacks are:

• Prairie Avenue:	Building setback	25 feet
	Parking setback	10 feet
• 104th Street:	Building setback	25 feet
	Parking setback	15 feet
• 102nd Street:	Building setback	15 feet
	Parking setback	10 feet
• Yukon Avenue:	Building setback	15 feet
	Parking setback	10 feet
• Cul-de-sacs:	Building setback	15 feet
	Parking setback	10 feet

UD2 The use of "defensible space" concepts in building and site design shall be encouraged to promote individual safety of site users. An area designed with the defensible space concept is an area which can be readily safeguarded against the occurrence of illicit activities. Parking lots, access ways, plazas and other public places must provide a comfortable ambience for the protection of public safety and security. Spaces are defendable when they are well lit, and when activities are viewed from both public and private vantage points. Through the orientation of buildings, streets, and access points, in addition to the provision of ample lighting, developments can be designed to deter the public from avoidable threats.

UD3 Where more than one lot is being consolidated into a single development, structures shall be developed as a "complex" of interrelated building masses, that visually convey the "sense" of a "village" center and as shown in Figure 10.

Century Boulevard

Prairie Avenue

Doty Avenue

Yukon Avenue

102nd Street

104th Street



BUILDING SETBACKS

- Prairie 25'
- 104th 25'
- 102nd, Yukon, Cul-De-Sac 15'

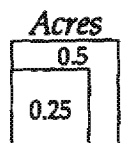


PARKING SETBACKS

- Prairie 10'
- 104th 15'
- 102nd, Yukon
- Cul-De-Sac 10'

FIGURE 8

**BUILDING AND VISITOR
PARKING AREA SETBACKS
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan**



ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates

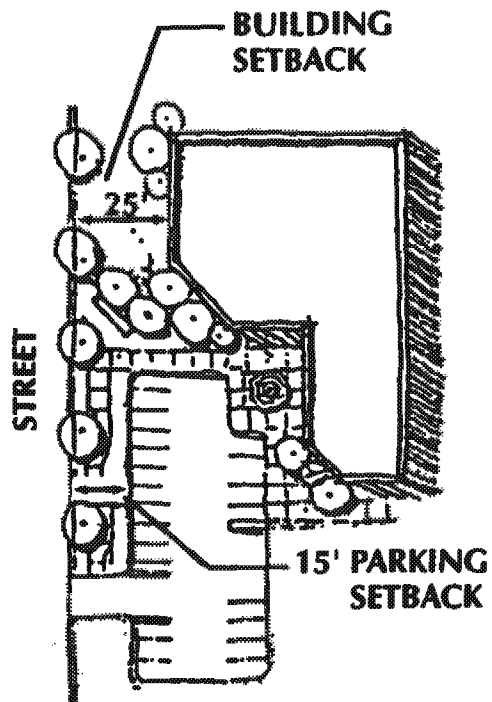


Figure 9
Example Visitor Parking Area Building Siting

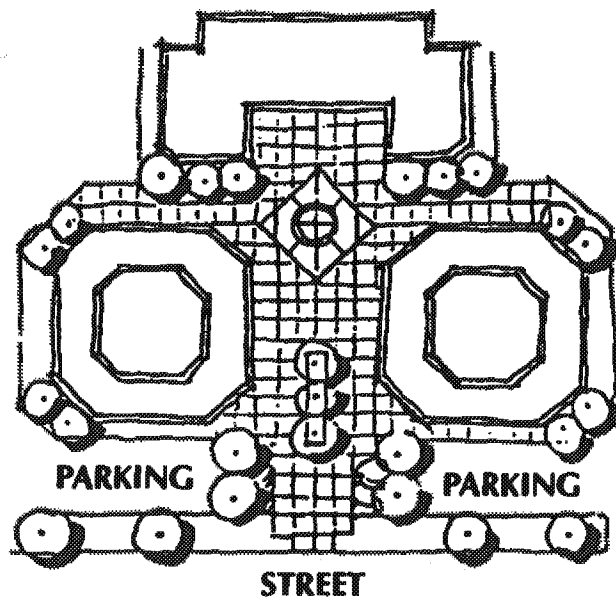


Figure 10
Example of Interrelated Building Masses
and "Village-like" Siting

A "village" sense should be conveyed through the use of such devices as building siting, wall articulation, cornice line, roof configuration, entryways linking with open spaces and plazas, and building materials.

Visitor Parking Areas and Driveways Siting

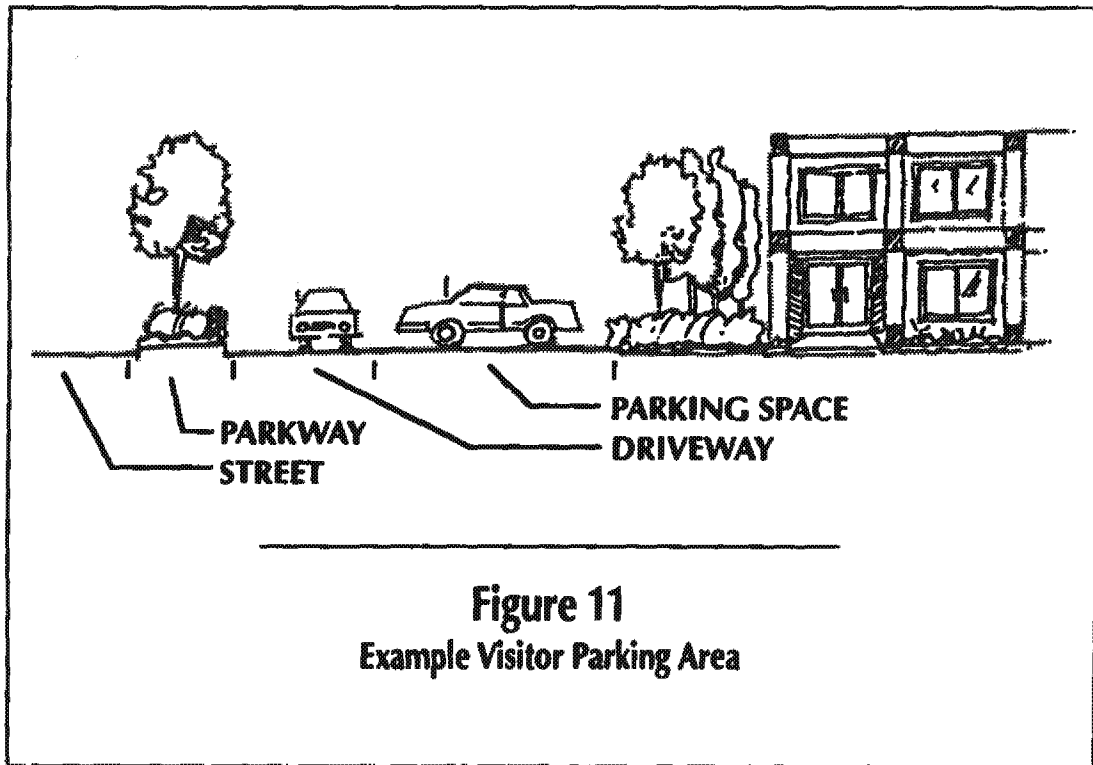
- UD4 Automotive driveways providing access to the visitor parking areas are required to be located and designed in such a manner as to minimize vehicular conflicts, and maximize site's aesthetic qualities as shown in Figure 11.
- UD5 Driveways shall be located no closer than 125 feet to another driveway or a street intersection. Exceptions are subject to review and approval from the City's Traffic Engineer and the Department of Community Development and Housing.
- UD6 Entry walks are encouraged to be separated from automotive driveways with landscaping as a buffer. The use of previous paving materials is encouraged, wherever feasible.
- UD7 Each lot should have no more than one driveway on 104th Street with a maximum width of 20 feet for automotive vehicle access to the visitor parking area.
- UD8 If a park is developed, the park shall contain a small, on-site parking lot in accordance with City of Inglewood Parking Standards. Parking lot access should have no more than one driveway on 104th Street with a maximum width of 35 feet.

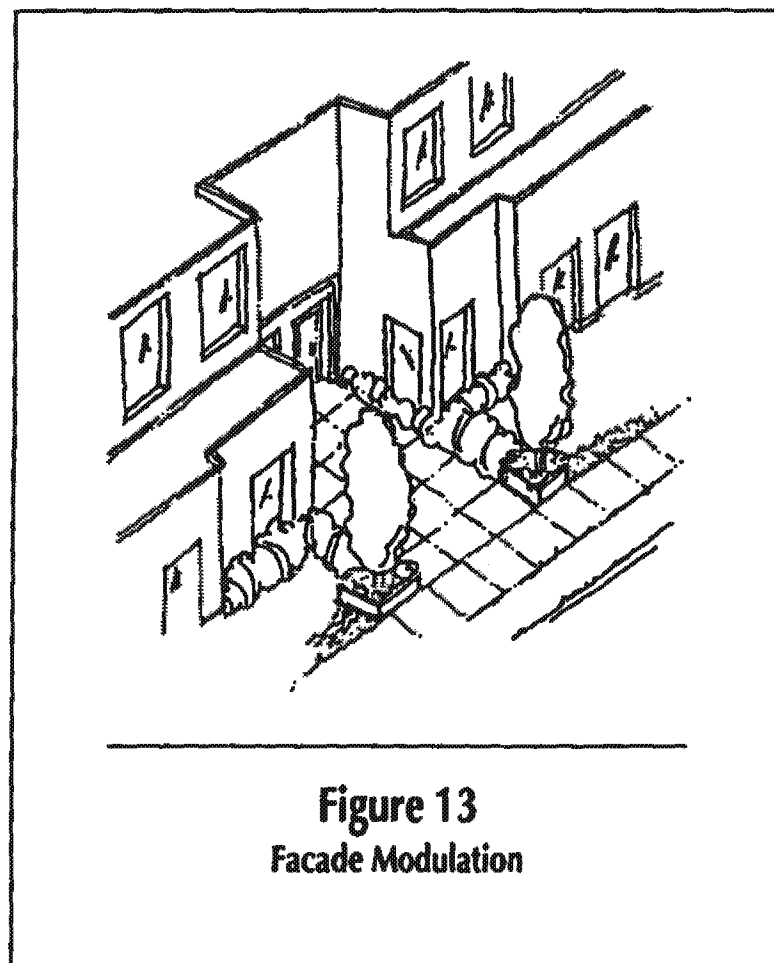
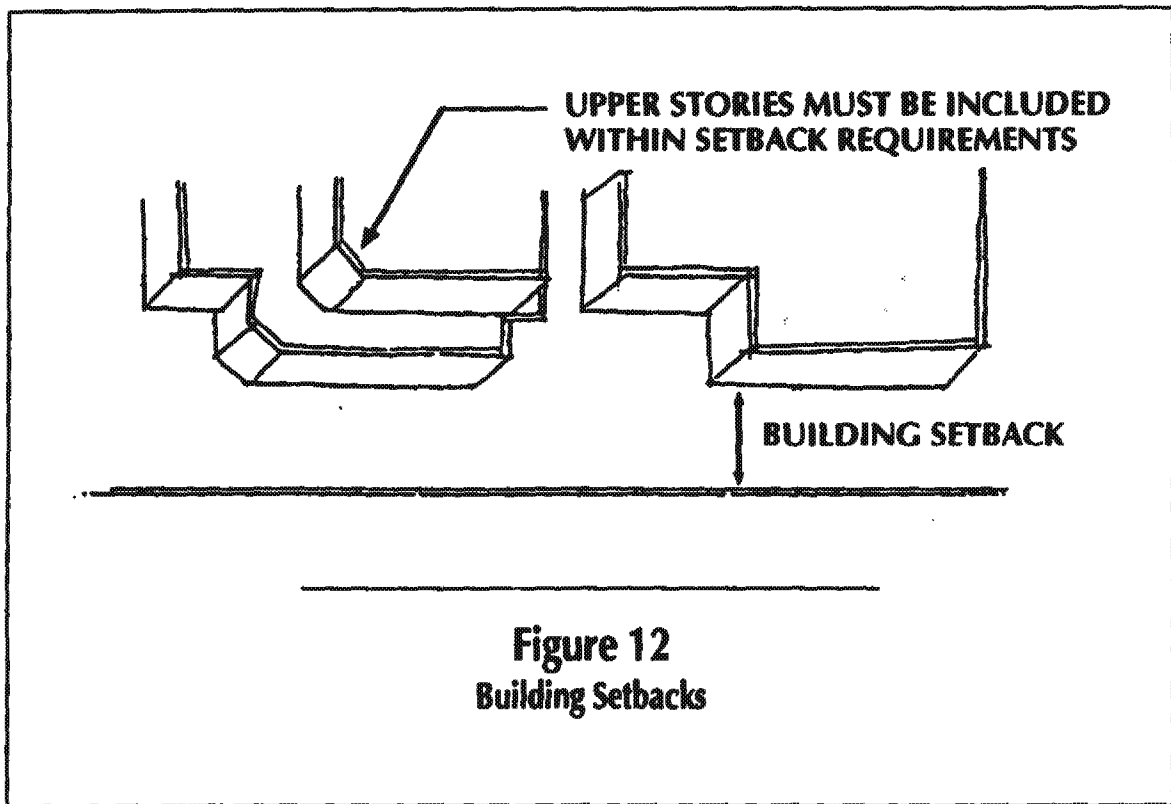
Parking Structures Design

- UD9 Where a parking structure is required to facilitate an insufficient surface parking supply, the parking structure is required to be in harmony with its built surroundings as described below:
- A) parking structures shall utilize similar colors and materials of the building(s) they intend to serve; and
 - B) parking structures shall be considered a building and be subject to limitations and setbacks as set forth within the IIBP Specific Plan.

Building Design and Architecture

- UD10 Buildings are required to be modulated both vertically and horizontally through the use of step-backs and changes in plane on elevations where visible from the public; the facade of the building shall not be a continuous linear plane as shown in Figure 12. To accomplish this a maximum of 65 percent of a building's facade shall be located on the setback dimension, the remaining 35 percent of the building's facade shall be setback at least 10 feet from the original facade line as shown in Figure 13.





- UD11 All buildings are to be attractively designed from all directions. Where the rear or sides of a building are visible from adjacent streets and alleys or from a nearby residential area, they shall receive equal design treatment as the front facade.
- UD12 Buildings fronting 104th Street are encouraged to be designed with lower heights or number of stories nearest the street. Building height, number of stories may increase as the distance from 104th Street increases, as shown in Figure 14.
- UD13 All buildings shall be designed in such a fashion that all ventilation, heating or air conditioning ducts, tubes, equipment, or other related appurtenances have been employed as an integrated and tasteful component of the design.
- UD14 The use of arcades, a belt course above the first building level, undifferentiated walls on the first level or base of the building, variation in building color and/or material, and building cornices are encouraged.
- UD15 Building articulation may include transparent and translucent windows, a patterned or scored building base (accomplished through the use of masonry block, smooth stucco walls deeply scored, applied color, or combinations of the above).
- UD16 Building design is encouraged to incorporate elements which minimize flat facades, such as recessed windows or alternating patterns of solid (walls) to void (windows) that will produce a "punched" window effect.
- UD17 The use of reflective glass or reflective metallic facades and other building material and colors which conflict with the overall context of development in the IIBP is limited, and subject to site plan review approval.
- UD18 Building materials and color should follow a pattern of earth tones and light greys. The predominant materials are masonry (brick and stone), stucco, and tile, or a combination thereof.
- UD19 Commercial buildings shall be designed to enhance pedestrian activity and convey "human scale" at the elevation(s), as follows:
- A) ground floor elevation of building facing the sidewalk must be visually and physically "penetrable", and incorporate architectural elements to provide interest and relief from flat surfaces (e.g., textured materials, offset planes, columns, recessed entries and windows, awnings) and compatible landscaping; and

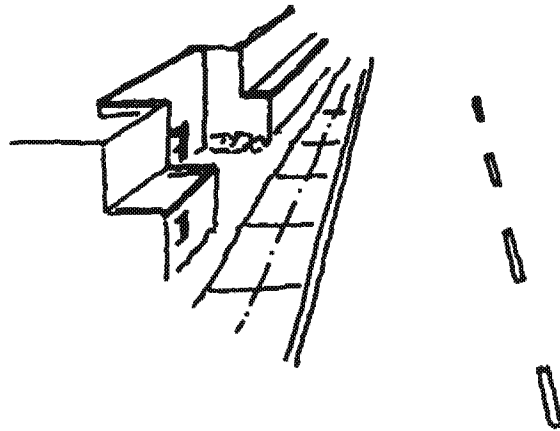


Figure 14
104th Street Building Facade Modulation

B) awnings, overhangs, arcades, trellises and other design elements which provide protection to pedestrians should be considered for use as shown in Figures 15 and 16.

UD20 Any building structure with expansion gates or security bars must locate gates or bars on the interior of windows or doors.

Fences or Walls

UD21 All walls shall be designed as a part of the overall site design and be constructed with materials that are compatible with the style of adjacent buildings; complementing finishes and colors.

UD22 Solid walls shall not be permitted within 20 feet of the right-of-way along lot lines. Any fence within 20 feet of the right-of-way shall not exceed a height of six (6) feet, and must be constructed with open ornamental work.

UD23 Walls located between the landscape setback area and building frontages may not exceed a height of three (3) feet if constructed of solid material, and may not exceed a height of six (6) feet if constructed with open ornamental work.

Building Signage

UD24 Private building signage is limited to the purposes of building, business, tenant, and address.

UD25 Wall, window, directory, and all other building signs shall be integrated with the building's architectural design, style and character.

UD26 All developments are to incorporate sign(s) location and sign(s) design as part of development plans subject to plan review.

UD27 All signage shall be a functional information system that contributes to the project's character in an unobtrusive manner. To achieve this, the following are required:

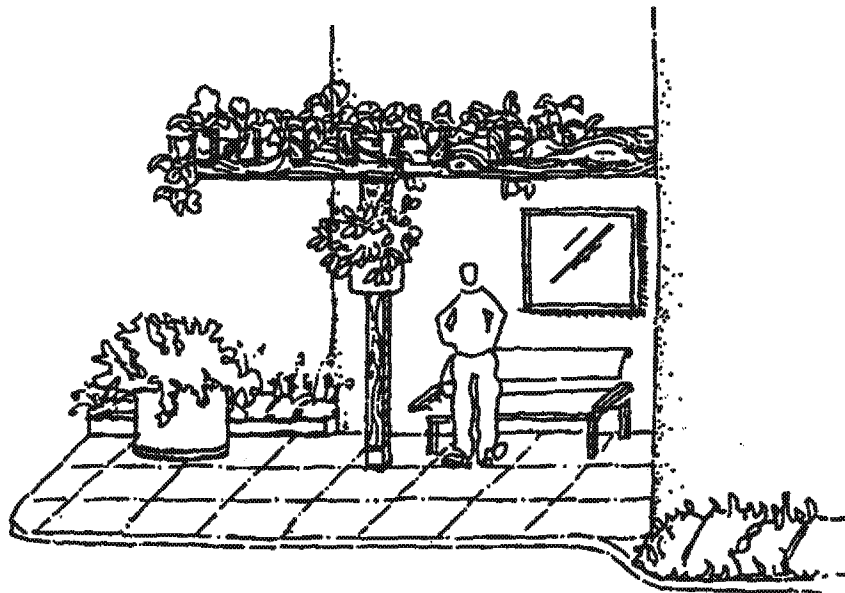


Figure 15
Examples of Design Elements to Enhance Industrial Building

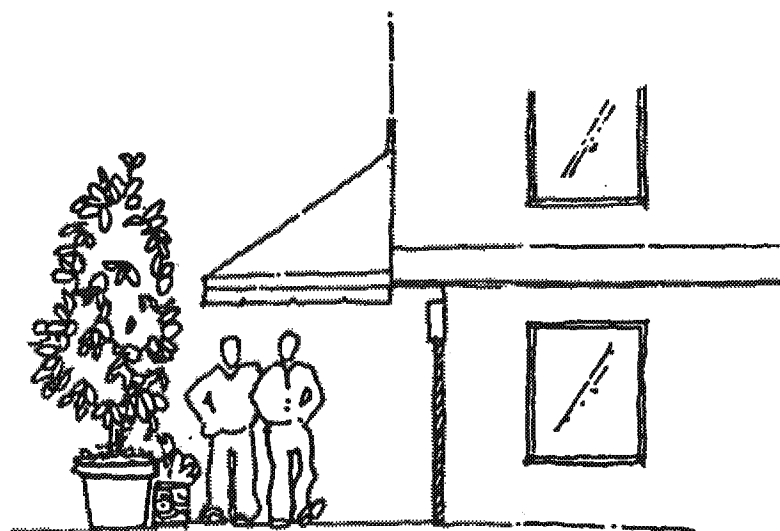


Figure 16
Examples of Design Elements to Enhance Pedestrian Activity and "Human Scale" on Commercial Building

Size

- A) the height of a sign is measured from the finished grade of the area surrounding the sign. The following design criteria represent maximum sign dimensions for development signage within the IIBP (refer to the Inglewood Sign Ordinance for specific sign definitions):

<u>Type</u>	<u>Maximum Height</u>	<u>Maximum Sign Area</u>
Horizontal Monument Sign	6 feet	100 sq.ft. in Park Overlay 108 sq.ft. in C-2 Zone 108 sq.ft. in M-1L Zone
Wall Mounted Sign	25 feet	50 square feet
Window Sign	24 Inches	10 percent of total window frontage
Pole Sign	Prohibited	Prohibited

- B) signs shall be limited to one (1) per street or parking lot frontage. An additional building identification wall sign for C-2 uses is allowed; the signs must be placed at least eight (8) feet above sidewalk grade or have a vertical dimensions not to exceed three (3) feet in height; and
- C) monument signs shall contain the name of the sole or private tenant, the multiple tenants' center name, or the name of the park.

Location

- D) signs should be placed to indicate access or the location of a business. Signs should be placed near the entrance of a complex or building to advise the most direct access to the business as shown in Figure 17;
- E) wall signs shall be placed to enhance facade character, scale and proportion. On buildings where facades are monolithic or plain, signs should establish or continue appropriate design character, scale and proportion;
- F) ground signs may be placed in building setback areas. Permanent signs which are parallel to the street shall be located a minimum of five (5) feet from the property line. When signs are placed in setback areas, care shall be taken to position the signs so as to maintain safe sight distances at entrances to the public right-of-way as shown in Figure 18; and
- G) building monument signs shall be constructed of a material compatible with the principle material found on the building it will serve. Monument signs shall be designed for and constructed with graffiti tolerant materials. Lettering shall be either carved into the sign wall or attached as individual letters. Lighting shall be external to the

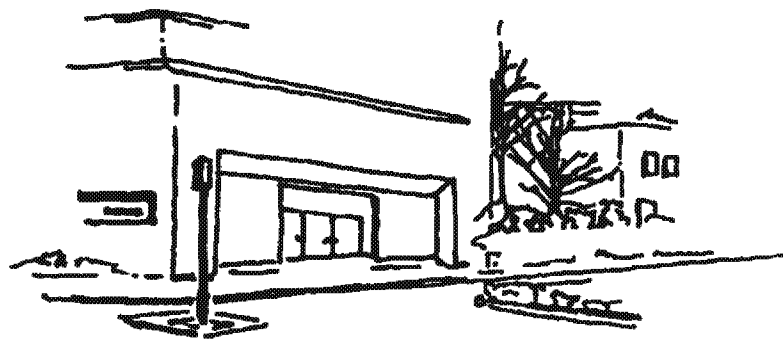


Figure 17
Building Entrance Sign Location

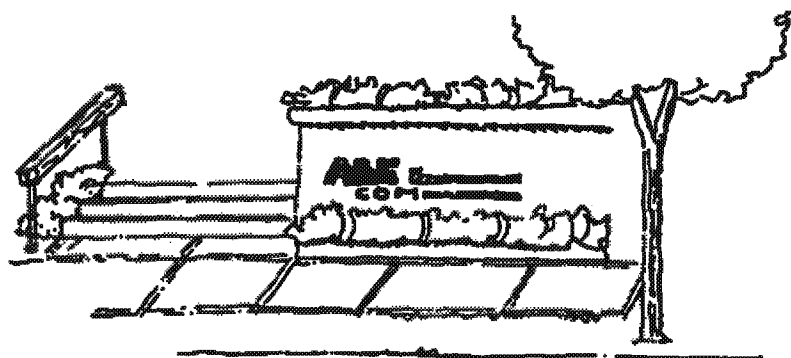


Figure 18
Permitted Ground Sign

sign or be incorporated into the backside of the individual letters to form a "light halo" effect on each individual letter. If the "halo effect" signs are used, the background of the sign must be opaque, and only letters and figures shall transmit light.

Color/Style

- H) sign messages should be brief for simplicity, readability, and overall attractiveness;
- I) signs should be in scale with their built environment. Forms, colors, and materials shall be carefully selected to blend development signage with the project's landscape and architectural theme;
- J) dark, neutral backgrounds should be utilized to contrast message letters and numbers in lighter colors;
- K) all logo images and type styles, on a given site, shall be accurately reproduced on all signage. Avoid hard to read typefaces that inhibits the sign's ability to communicate its message. Messages are most easily read when lettering uses the upper and lower case form; and
- L) signs consisting of any moving, revolving, flashing, blinking, or otherwise animated lights shall be prohibited in the industrial area. Animated signs are permitted in the commercial area subject to site plan and design approval.

Materials

- M) materials such as sheets of plastic or other easily vandalized materials are discouraged; if used they must be designed to emulate the style, materials and colors typical of the project development.

Illumination

- N) illumination of signage shall be unobtrusive yet visible for night viewing. Permanent signage may be illuminated by front lighting, silhouette lettering, or internal illumination.
 - a. Front Lighting - Front lighting is permitted for monument signs. Front lighting shall be evenly dispersed over the surface of the entire sign area. The intensity and color of lighting for the sign should match that of the projects' lighting. Additionally, the lighting fixture must be secured against vandalism.
 - b. Silhouette Lighting - Reverse channel letters may be back-illuminated with low-voltage lights or neon to create a silhouette effect. Letters may not transmit light.
 - c. Internally Illuminated Lighting - The background of the sign must be opaque, and only letters and figures shall transmit light.

Street/Site Furniture

- UD28 Street/site furniture shall be installed where appropriate, or as needed, where it does not impede pedestrian or vehicle physical or visual access to buildings, where it is aesthetically pleasing, consistent in design, functional, durable, and is conducive to pedestrian activity and discourages sleeping on benches. Street/site furniture includes bus shelters, trash receptacles, newspaper racks, bicycle racks, public telephones, and drinking fountains.
- UD29 Buildings with patio areas shall provide appropriate seating, trash receptacles, and drinking fountains.
- UD30 The relative size and design of the street furnishings should be compatible with the buildings to which they relate.
- UD31 All street furniture should be colored in a neutral tone to blend with a mix of backgrounds, and be flat and non-glossy to produce a uniform appearance even when applied to various materials.
- UD32 Street furnishings should be constructed of durable, easily maintained materials that will not fade, rust, rot or otherwise deteriorate.
- UD33 Cluster similar-function serving furniture with benches (i.e., trash receptacles, water fountains).
- UD34 Incorporate improvements including street furniture, street light fixtures, sidewalks, public signage, and landscaping, along 102nd Street, 104th Street, Yukon Avenue, and Prairie Avenue to distinguish these streets as being part of the IIBP area.

Public Signs

- UD35 All existing public signs (i.e., stop and speed limit signs) which do not conform to the following design standards should be replaced:
- A) traffic control signs are to be designed to conform to the Federal Manual of Uniform Traffic Control and the Cal Trans Manual of Uniform Traffic Control Devices;
 - B) public sign placement shall conform to local standards. Free standing signs should be mounted and painted as the standard street furniture; and
 - C) minimize the number of sign posts by placing signs on standard street light modules. Where not possible, signs may be installed on a new sign post structure.

Billboards

- UD36 The City shall consider the acquisition and remove existing billboards in the IIBP upon lease expiration and as funding is available. State law permits a city to purchase, in fee, billboards.
- UD37 The installation of new billboards in the IIBP is prohibited.

Entry Monuments

- UD38 Major entry monuments on both corners of the 102nd Street at Prairie Avenue and 104th Street at Prairie Avenue intersections shall be provided by the project developer. A secondary entry monument located at 102nd Street and Yukon Avenue shall also be provided. Corner sites are required to install the sign and to grant a triangular easement or possibly dedicate the monument sign area to the City. The City shall determine method at the time of development project review.
- UD39 Major entry monumentation shall be placed at a 45 degree orientation to Prairie Avenue at the 102nd Street and the 104th Street intersections, and within the landscaped setbacks of the IIBP project area as shown in Figure 19.
- UD40 Minor entry monumentation shall be placed at a 45 degree orientation to Yukon Avenue at the 102nd Street and the 104th Street intersections, and within landscaped setback areas of the project site.
- UD41 The project signage should be horizontal in form and surrounded by plantings of low evergreen shrubs and annual color to accentuate the signage face. Evergreen trees should be used as a backdrop for the monument signs as shown in Figure 20. All signage and plantings should be designed and installed in a graffiti tolerant manner.
- UD42 The ground, surrounding the monument signs, may be bermed, and walls with signing attached may be stepped, if necessary, in order to elevate the signs. Berming shall be limited to 2 feet in height above the surrounding finished grade. All materials must be graffiti tolerant.
- UD43 The monument signs shall be front lighted, and all lighting hardware should be concealed from public view through the use of planting materials.
- UD44 All project identification signage should be compatible and complementary to each other.

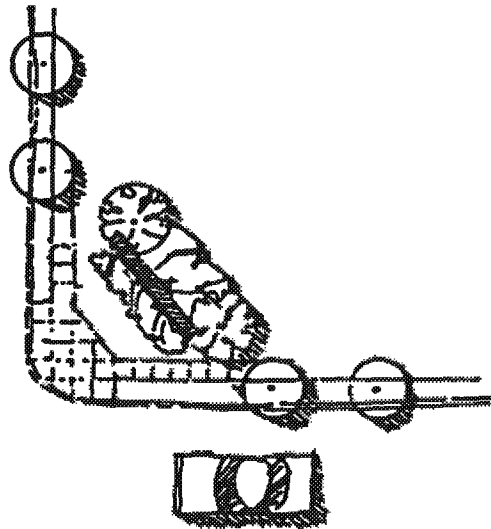


Figure 19
Entry Monument Sign Placement

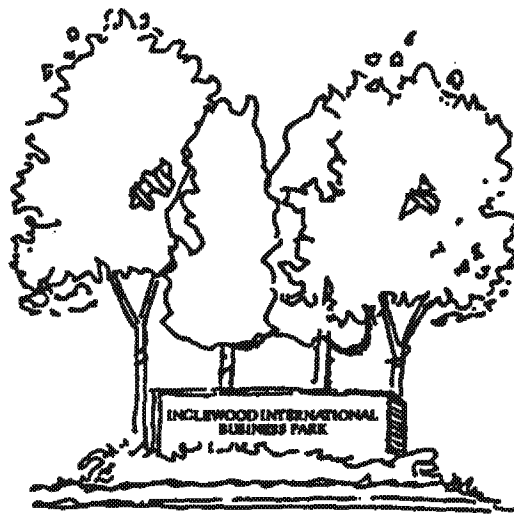


Figure 20
Entry Monument Sign Landscaping

Sidewalks

UD45 Sidewalks are to be constructed along all street frontages in the IIBP area. Link sidewalks with private development walkways and plazas. Standards for sidewalks are as follows:

- A) constructed to a minimum paved width of 6 feet, and
- B) all paving repairs should be accomplished with "in-kind" materials and in a timely manner.

The use of pervious paving materials are encouraged wherever feasible.

UD46 Require 104th Street sidewalks to undulate at variable distances from the street curb line. Permit sidewalks on Yukon Avenue, Doty Avenue, Prairie Avenue, and the IIBP cul-de-sac streets to meander.

A 2 to 4 foot parkway shall exist from the street's curbline to the edge of sidewalk. Sidewalks should meander to create the 2 to 4 foot parkway width, and may not be located at the 2 foot setback for more than 40 percent of the linear street frontage for each lot as shown in Figure 21.

Street Trees

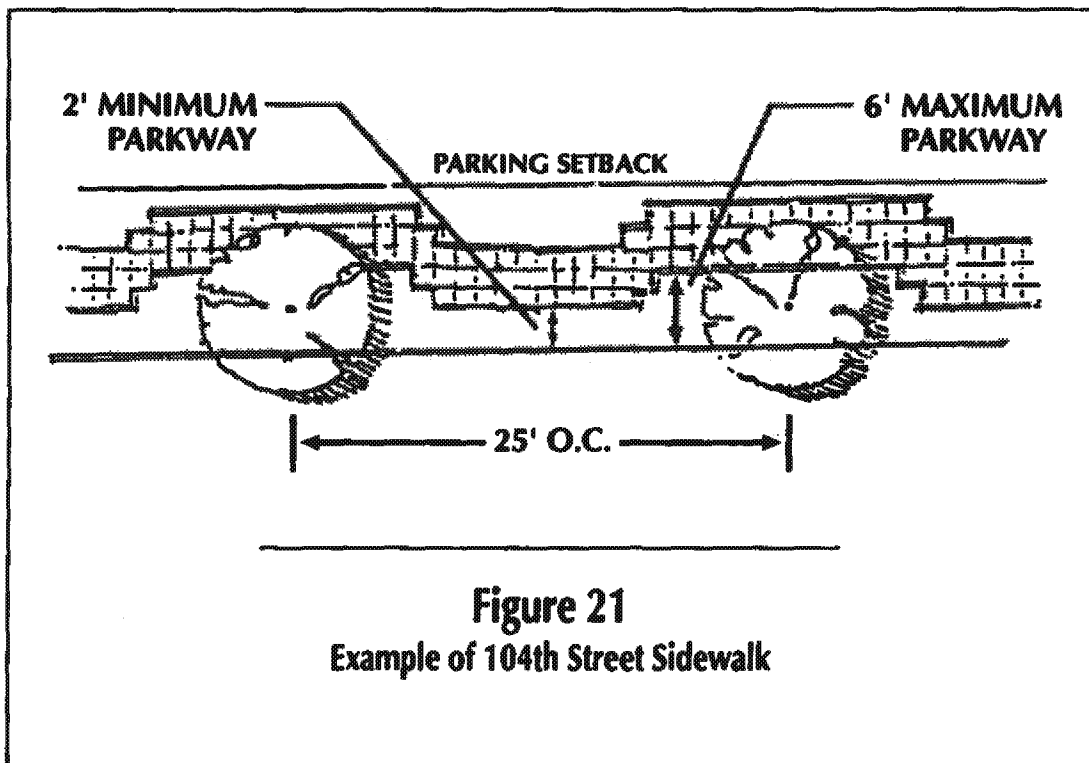
UD47 The City shall allocate funds for the installation of street trees along street frontages where they have been removed or nonexistent and when it is anticipated that there will be no development activity which would result in their installation. Revenue sources may include the General Fund, exactions from developers, streetscape assessment district.

Landscaping

UD48 Property setback areas exclusive of walkways, driveways, and paved plazas are to be fully landscaped with a palette of vegetation that should include a combination of lawn, ground cover, water efficient plant materials, and trees and shrubs in a hydrozone scheme.

UD49 Trees located along all street frontages and scattered throughout the site to provide shade, visual unity, and color. Selected tree species shall be drought-and smog-tolerant, fire resistant, and pest resistant. Preserve and protect existing trees and shrubs, to the extent possible.

UD50 New street trees shall be a minimum of 36" box size (and be planted in a linear fashion spaced 25 feet on center), where site conditions do not permit a 36" box size tree, the new street tree shall not be smaller than 24" box size.



- UD51 Automatic underground irrigation systems are required. The use of reclaimed water is encouraged.
- UD52 All exterior portions of each parcel including landscaped areas, walks, parking areas and service areas are to be kept groomed, maintained, and free of litter and debris.
- UD53 Irrigation shall be zoned by plant community and shade/sun patterns to ensure proper watering.
- UD54 The location of irrigation backflow devices and controllers are to be placed in areas of minimum public visibility or screened with appropriate planting materials.
- UD55 Irrigation installation shall minimize hazards to pedestrians. Irrigation heads adjacent to walks, drives and curbs shall be of the pop-up type.
- UD56 Shrubs, ground covers and flowering plants in both at grade and planter boxes are required along the front setback and adjacent the buildings. Landscape plant materials in containers providing seasonal changes of color are encouraged.
- UD57 The use of turf materials is discouraged unless the planted area is greater than 100 square feet and wider than five feet. Turf materials, if used, should be warm season grasses, hybrid fescues or lawn substitutes.
- UD58 Trees shall be planted in the parking lots. The types of trees to be planted are contingent on their coverage at maturity. Ten years after planting, tree specimens shall provide shade cover of 40 percent or more of the parking surface.
- UD59 Notwithstanding these specifications, all plant materials and landscape equipment shall comply with the Landscape Ordinance, Chapter 12 of the Municipal code.
- UD60 All landscaping materials which die or are removed are to be replaced "in kind" and in a timely manner.

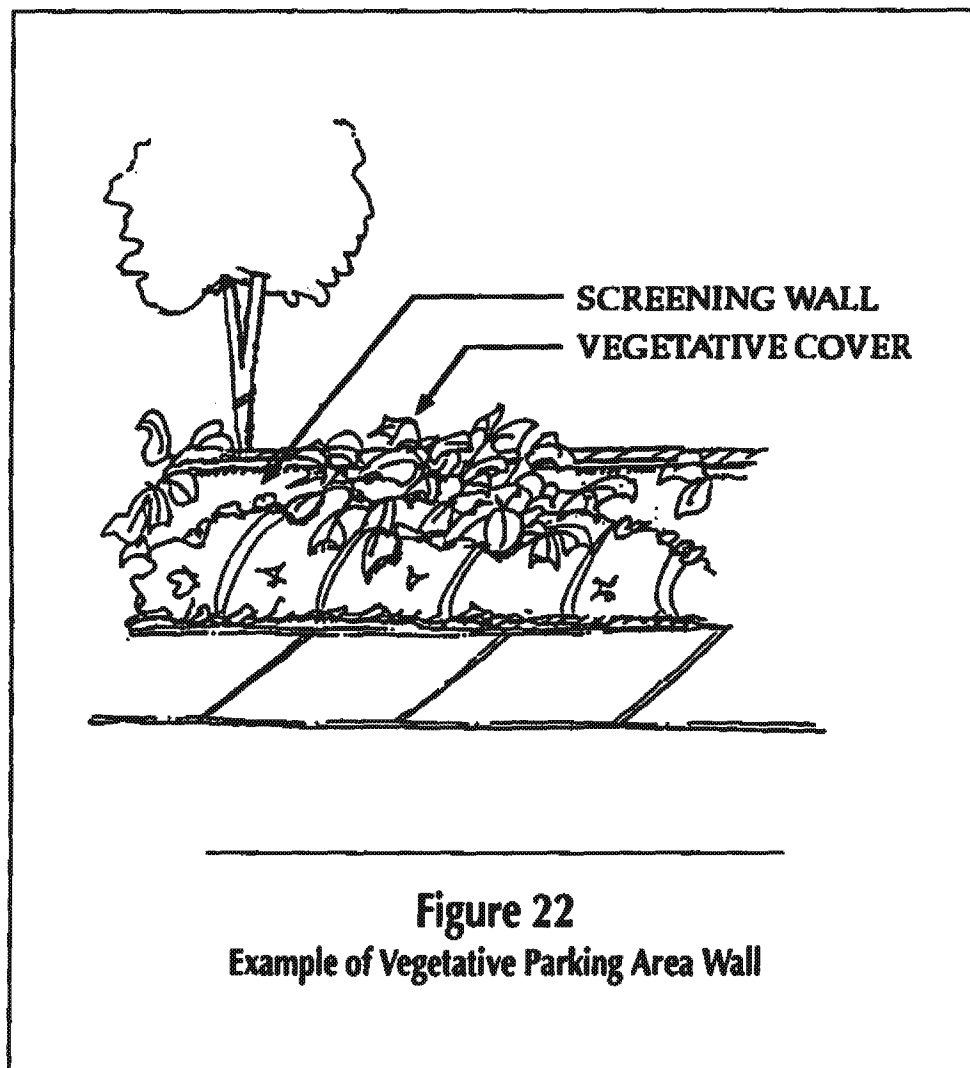
Visitor and Park Parking Area Walls

- UD61 Visitor and park parking area walls are to substantially screen automobile from public view. Height of screens may not exceed three (3) feet if constructed of solid materials, and not exceed six (6) feet if constructed with open ornamental work.

- UD62 Vegetative covers, such as vines and shrubs, are to conceal the face of walls and to lessen opportunities for graffiti as shown in Figure 22.

Underground Utilities

- UD63 All new development or adaptive reuse projects are required to underground new and existing utilities, and provide risers at the property line for hookup to existing above-ground utility infrastructure.
- UD64 The City shall develop and implement a program to underground utilities within utility easements, and within public right-of-way.



The purpose of the Circulation, Site Access, and Parking development standards is to provide a street network that adequately provides access without impacts on surrounding roadways, provide an on-site circulation system that utilizes 102nd Street and the IIBP cul-de-sacs for truck and employee access and parking, and provides visitor access on 102nd Street and 104th Street.

Standards

- C1 All developments must analyze their specific projects' impacts and adequately mitigate any impacts.
- C2 102nd and 104th Streets shall be widened as follows:
 - southern side of 102nd Street widened 6 feet; and
 - northern side of 104th Street widened 15 feet as depicted on Figure 7.
- C3 The IIBP appendage streets shall be 60 feet wide with a 42 feet radius cul-de-sac at the southern terminus of each appendage street as depicted on Figure 7.
- C4 On street parking is prohibited on the south side of 102nd street.
- C5 The Prairie Avenue/102nd Street intersection shall be signalized.
- C6 The Prairie Avenue/102nd Street intersection shall be restriped to permit dual westbound left turn lanes.
- C7 The Yukon Avenue/102nd Street intersection shall have additional stop signs controlling northbound and southbound Yukon Avenue traffic.
- C8 Trucks are permitted to use Prairie Avenue, Century Boulevard, Imperial Boulevard, and 102nd Street from Prairie Avenue to Yukon Avenue.
- C9 Prohibit truck travel on 104th Street west of Yukon Avenue, and on Yukon Avenue south of 104th Street.
- C10 No overnight truck parking is permitted on IIBP public streets or access easements.
- C11 Primary parcel access shall be from 102nd Street and the internal roadways (cul-de-sacs and access easements).
- C12 Sites fronting 104th Street are to derive all trucking and most employee access and parking from cul-de-sacs, and provide a separate visitor parking area on 104th Street.

- C13 Parcels requiring access from designated access easements and parcels adjacent to access roads must obtain reciprocal parking and access agreements from parcels providing the access easement.
- C14 A commercial driveway shall not be closer than 150 feet to another commercial driveway.
- C15 Developments shall include interconnected sidewalks and handicap ramps linking adjacent sites as well as transit facilities.
- C16 All public and private developments must provide sufficient on-site parking to meet their generated demand.
- C17 All public and private developments must comply with city parking requirements.
- C18 The first parking stall which is perpendicular to a driveway, or the first aisle juncture from the access point should be a minimum of 30 feet from the driveways curb.
- C19 Commercial parking requirement reductions are permitted for M-1L developments with office uses. The parking requirements for M-1L developments with office uses are as follows:

<u>Industrial Square Feet</u>	<u>Requirement</u>
0 - 5,000	One space for each 500 sq. ft. of industrial gross floor area.
5,000 - 15,000	Ten spaces, plus one additional space for each additional 2,000 sq. ft. above 5,000 sq. ft.
15,000+	Fifteen spaces, plus one additional space for each additional 1,500 sq. ft. above 15,000.
PLUS	
<u>Office Square Feet</u>	<u>Requirement</u>
0-15,000	One space for every 300 sq. ft. of office space
15,001 +	One space for every 300 sq. ft. of office space below 15,000 of office sq. ft., and one space for every 360 sq. ft. of office space above 15,001 sq. ft. of office use.

- C20 Developments with more than 100 anticipated employees must participate in a Transit Management Organization (association) and must employ a TDM coordinator to establish and monitor the program. Developments with more than 100 anticipated employees are to provide vanpool parking stalls in the employee parking area.

The intent of the Public Works and Services development standards is to ensure that the adequate level of service currently provided to the IIBP continues to be available as the IIBP changes land uses and perhaps changes intensity of use for water, wastewater, storm drainage and solid waste collection, fire, police, and emergency service.

Standards

- PW1 New public and private developments are required to prove that adequate water, wastewater, and storm drainage facilities will be available and operational prior to the issuance of any Building Permits and/or Certificates of Occupancy.
- PW2 Street lighting shall be oriented and compatible with street furniture, provide lighting which illuminates pedestrian areas for security and visibility, and prevent lighting impacts on adjacent residential uses. Standards for street light mounting poles are:
- A) Prairie Avenue: 30 feet; and
 - B) Yukon Avenue, 102nd Street, 104th Street and IIBP Cul-de-sacs: 15 feet.
- PW3 Base fixture spacing and all photometric data for lighting performance on American National Standard Practice for Roadway and Walkway Lighting.
- PW4 Pedestrian areas shall have a minimum of 15 foot candles.
- PW5 Park lighting shall be oriented to minimize light and glare impacts on surrounding residential areas and shall be in accordance with the City of Inglewood standards.
- PW6 New public and private development projects are required to pay the cost of necessary public improvements (in accordance with City of Inglewood standards) created by the project, either through the assessment of impact fees, or by the actual construction of improvements in accordance with State Nexus Legislation.
- PW7 All developments within the IIBP shall utilize sound construction techniques and state-of-the-art water saving fixtures, seismic, and thermal design standards; all structures are to be constructed in accordance with City's building and other pertinent codes and regulations.
- PW8 All structures are to be designed and constructed with secondary building exits to allow for the safe and timely evacuation of occupants in the event primary exits become blocked or impaired during a disaster.

- PW9** Projects that will excavate 50 feet in depth or more should be reviewed by the Public Works Department, Metropolitan Water District and other applicable agencies to preclude groundwater resource impacts.
- PW10** All hazardous materials are to be on record with the Inglewood Fire Department, prior to the issuance of the Certificate of Occupancy.
- PW11** Upon development of the auto shop parcel, a study shall be performed to determine if 1) underground storage tanks have existed or are present, and 2) soil remediation is necessary.
- PW12** Use of all hazardous materials and production of hazardous waste shall be in compliance with existing City law and the Hazardous Waste Element.
- PW13** All vacant sites and construction sites shall be completely enclosed with fencing material and adequate nighttime lighting, in accordance with Police Department approval.

The intent of the Environment development standards is to insure that any excessive noise, excessive air quality pollutants, and excessive light and glare generated by the IIBP or its uses be evaluated and mitigated to acceptable levels.

Standards

Noise

- E1 Any environmental studies/reports completed within the IIBP area shall include a noise impact evaluation as well as any proposed mitigation measures needed to alleviate unacceptable noise levels experienced or to be experienced. The noise impact evaluation shall at a minimum include the following:
- A) illustrate the area's existing and projected ambient noise levels as well as their sources;
 - B) calculate the projected reductions to interior noise levels;
 - C) evaluate impacts on adjacent land uses; and
 - D) illustrate the proposed sound attenuation features, if any, to be incorporated into the project's design which are needed to meet the required interior noise level reductions.
- E2 Development generateing ambient noise levels that exceed 65 dB(A) at the site's property line is to incorporate special sound attenuation features into project design to reduce interior noise levels. Complete conformance with the measures required by the Uniform Building Code, FAA guidelines, and the State Noise Insulation Standards (California Administrative Code, Title 24) shall be required.
- E3 Buildings are to be designed and oriented as to minimize potential for noise impacts on surrounding residential uses.

Air Quality

- E4 Land uses generating significant levels of stationary emissions and contributing to the severe degradation of local air quality shall be restricted. Uses must mitigate significant impacts to a non significant level.

Light and Glare

- E5 All developments are to provide exterior lighting that is adequate, ensures a safe environment without the nuisance of light or glare impacting adjacent properties and is in accordance with standards outlined in the Urban Design section.

- E6** The lighting of the vertical face of buildings rather than the horizontal foreground to ensure greater security and a lessened glare impact on neighboring uses is encouraged.
- E7** The use of energy conservation in developing lighting plans, using the most efficient and effective technical and operational energy conservation concepts are encouraged.
- E8** All developments are to provide and locate streetscape lighting within the public right-of-way.

The Implementation section of the IIBP Specific Plan is comprised of four tools: 1) A resolution for adopting the Specific Plan, 2) Funding Program, 3) Public Involvement Program, and 4) Minority Business Enterprise Involvement Program.

Resolution

The resolution to be adopted by the Inglewood City Council to enact the IIBP Specific Plan will be prepared by the Community Development and Housing Department of the City of Inglewood.

Funding Program

It shall be the policy and the implementation responsibility of the City of Inglewood:

1. To maximize investor, owner, and developer financial performance in the development of industrial properties in the Specific Plan Ordinance area.
2. To limit the City's financial participation in improvement of necessary traffic safety and circulation improvements in the public rights-of-way, and other such public improvements in public rights-of-way or in granted across private properties which are necessary for fire protection, storm water management, wastewater lines or other City of Inglewood Utility responsibility.
3. To limit private property purchases to those instances wherein public rights-of-way are required, or to circumstances of willing seller-willing buyer wherein unique opportunities for land assembly occur.
4. In any instance in which the City may purchase property, it shall be responsible for providing tenant relocation advisory services and payment of relocation benefits in the same manner and via the same eligibility and benefit schedules as specified in the Federal Uniform Relocation Act. The City shall require property owners whose property it purchases to carry out their own relocation and the City may provide relocation advisory services to such owners at its discretion.
5. The City of Inglewood is not obligate to buy or to sell private properties in the Specific Plan area except as may be required for public purposes of rights-of-way development. Any other private property purchase shall be at the City's sole discretion.
6. The City may sell or lease any properties purchased for fair market value for feasible reuse as private industrial or commercial reuse consistent with the land use designations and allowable uses contained in the Specific Plan. The determination of fair market value shall be made according to the normal standards in the marketplace, including residual value analysis.
7. The City shall impose all normal and standard subdivision tract map and land division requirements upon the developers of industrial and commercial reuse properties in the Specific Plan area, including appropriate requirements for public improvements necessitated by the private project.
8. The City may use, at its sole discretion, eligible public funds for the implementation of the Specific Plan as may be necessary from time to time.

9. Sources of funding for the implementation of the Specific Plan and for any public-private development agreements which are consistent with the purposes of the Specific Plan Resolution and are in conformance with the development standards set out in this ordinance may include, but not be limited to, the following:

- City General Fund appropriations;
- Redevelopment Agency payback of City loans made to other Redevelopment Project Areas;
- Capital Improvement Program funds;
- Redevelopment Agency cost participation which is of direct benefit to the Century Redevelopment Project;
- FAA and City of Los Angeles Department of Airports airport noise abatement/use conversion funds;
- Other grant and/or loan funds as may be available from time to time;
- Proceeds from the sale or lease by the City of properties returned to private reuse within the ordinance area; and
- Loans and payments to the City from private developers which are a part of a public-private development agreement approved by the City for industrial or commercial reuse within the ordinance area.

Public Involvement

It shall be the policy and implementation responsibility of the City of Inglewood:

1. To hold a publicly noticed annual meeting for the owners, tenants, and residents of the Specific Plan area to describe progress and implementation prospects as well as provide direct public access to information, for a term not exceeding 10 years from the effective date of the Specific Plan Resolution.
2. To publish from time to time, but not less often than once a year, a newsletter concerning the implementation progress of the Specific Plan—to be mailed to each owner of record, business tenant, and residential tenant. A summary in Spanish shall also be provided.
3. All zone changes, subdivision actions, and other matters requiring noticed public hearings regarding property development in the ordinance area shall be provided to the surrounding property owners as per the existing perimeter radius notification requirements of the City. The City shall also notice the property owners in the ordinance area.

Minority Business Enterprise Involvement

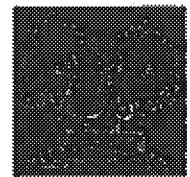
It shall be the policy and the implementation responsibility of the City of Inglewood:

1. To extend offers of first right to assemble properties for industrial development to the existing property owners, in the first year after the Specific Plan is adopted.
2. To provide existing business owners and business tenants with the opportunity to reestablish businesses within the Specific Plan area, depending upon their capabilities and the industrial business type compatibility.
3. If the City or the Agency is involved in any property purchase--to extend an offer of relocation advisory service and limited relocation benefits to the selling owners and their tenants.
4. To offer Specific Plan area property sellers (who sell to the City) opportunities to be developers, buyers, or tenants when such opportunities become available within the City's existing, ongoing redevelopment projects. Such opportunity shall be available for one year after City purchase of property in the Specific Plan area.
5. To establish Minority Business Enterprise (MBE) opportunity as one of the purposes of the Specific Plan and require City management to report upon results annually to the City Council over a minimum 10-year period from date of ordinance adoption.
6. To provide guidelines to developers and property managers which encourage the provision of opportunities to MBE firms in the following areas:
 - As professional design, engineering, and development consultant firms on the developer team;
 - As development equity investors;
 - As tenants in developing projects; and
 - As ongoing business services provider (accounting, auditing, building and grounds maintenance, courier/delivery services, etc.).
7. To provide annual MBE opportunities matchup sessions for developers, for tenants, and for owners of Specific Plan area properties.
8. To provide notice of opportunities for business to the City's list of MBE establishments on a regular basis, as developments occur and as initiatives are marketed by the City in the Specific Plan area.

Responsibility for implementation of this section shall lie with the Office of the City Manager or such department as may be formally delegated by the City Manager.

Appendix A

Existing Conditions



INGLEWOOD INTERNATIONAL
BUSINESS PARK

EXISTING ZONING, GENERAL PLAN DESIGNATION AND LAND USE

Historically, the area was zoned C-2 along Prairie Avenue and R-3 on 102nd Street, 104th Street, Doty Avenue, and Yukon Avenue. In 1979 and 1980, the area was rezoned to Light Manufacturing (M-1L) as a result of high air traffic noise levels. Due to the zone changes, all but three of the existing land uses within the area do not conform with the present zoning nor the Industrial General Plan designation.

The 44.4-acre area contains 115 parcels. The typical parcel is a long, thin rectangle containing approximately 30,400 square feet. Approximately 29 percent of the lots are owner occupied. With the exception of apartment buildings, structures in the area are single story. The land uses in the area include:

- residential (single family residential, lower density multiple family ("duplex-style"), and mid-density multiple family residential "apartment complex");
- commercial;
- industrial;
- schools and churches; and
- abandoned/vacant.

A complete list of study areawide existing land uses are presented in Table A-1 and pictorially shown in Figure A-1. A lot by lot listing of existing land use is presented in Appendix A. Land use descriptions are presented in Table A-2.

Residential is the most prevalent use within the area; 98 parcels are used for residential purposes. On the 98 parcels are 657 residential units. Of the 657 units, 42 are single family residential units, 145 are duplex style units, and 470 are apartment units in 18 complexes. Single family units and lower density multiple family units are dispersed throughout the area.

Two single-story commercial uses are located in the area on Prairie Avenue. The first commercial use contains six retail shops catering to a Hispanic clientele, and the second is developed for a restaurant. The two sites comprise approximately 12,000 square feet of commercial space. Off-street parking serving the retail shops is limited; most customers who drive to the shops park on the street. The commercial structures appear to be in fair condition, however, the diversity of paint color and the numerous signs cause visual disharmony.

A school and church are located near the 104th Street and Prairie Avenue intersection. A day care facility is located on 104th Street between Yukon Avenue and Doty Avenue. At the time of the land use survey, none of the facilities were in use. Both the school and the church appear to be in good condition.

Industrial uses occur in three locations on 102nd Street. The 81,000 square feet (approximately) of industrial use is primarily storage facilities, and small manufacturing. The industrial structures are new and are in good condition.

TABLE A-1**Existing Land Uses**

		Acreage	% of Total
Residential	Single-Family	6.84	16
	Low Density Multi-Family	11.28	25
	Higher Density Multi-Family	10.73	24
Commercial	Retail	.28	1
Industrial	Light Industrial	1.86	4
Institutional	School/Church	3.60	8
Public Right-of-Way		7.74	17
Vacant		2.08	5
Total		44.41	100



**EXISTING LAND USE
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

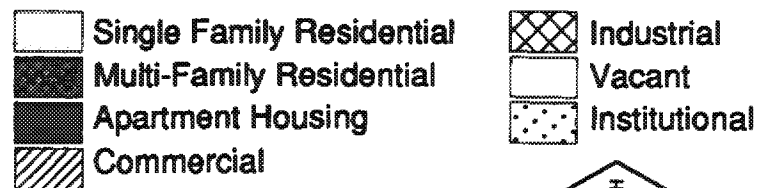


FIGURE A-1

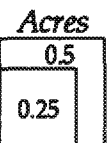


TABLE A-2**Land Use Definitions**

Residential	Single Family - One detached dwelling unit per lot. Lower Density Multiple Family - Two to six units per lot in a duplex style structure. Higher Density Multiple Family - Seven to 61 units per lot in an apartment complex style structure.
Commercial	Retail - Food stores, general merchandise, beauty salons, restaurant.
Industrial	Light Industrial - Freight air cargo, plumbing, sheet metal, etc.
Institutional	School, church, day care.
Public Right-of-Way	Roadways
Vacant	Lands not involved with economic use.



**EXISTING STRUCTURES
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**



GENERALIZED BUILDING FOOTPRINTS

INDUSTRIAL LOTS

FIGURE A-2



0 50 100 150 200 250 Ft.

Acres
0.5
0.25

Seven lots within the area are vacant. These vacant lots (containing no structure or unused structures) are primarily clustered on Prairie Avenue. All vacant parcels are in poor condition with litter and abandoned items piled on-site.

In general, the land uses in the area are inappropriately mixed. For example, a defunct autobody shop is adjacent to residential structures. Similarly, the commercial uses are adjacent to residential resulting in pedestrian/resident/vehicular conflicts created by the mixed use.

BUILT ENVIRONMENT

Urban design is the collective visual and environmental character of an urban or developed area. Conceptually, its quality depends on the sum of the qualities of its elements. The elements include:

- physical condition of existing structures;
- streetscape (surface materials of sidewalks, streets, street furniture, utility poles); and
- design linkages (the fit of differing architectural designs and building scales).

The architectural image of the area is one of nonharmonious diversity. Residential uses vary from small-scale, bungalow style homes, to newer, "boxy" apartment complexes. The industrial uses are new, tilt up slab concrete structures. Each industrial lot contains on-site parking which services that lot. Fences along the front and side property lines impair if not prohibit the industrial patrons and workers from interacting and linking with any surrounding uses. Photoplates A-1 through A-3 are examples of architecture found in the area. Figure A-2 shows existing structures' outlines (or building footprints).

The overall visual appearance of the area is cluttered, messy, disharmonious, and chaotic; although, some structures and goals are will maintained. The contributing negative conditions in the area include:

- piled rubbish, discarded furniture, and litter;
- lack of landscaping or poor landscape maintenance;
- lack of or poorly maintained parkways;
- abundance of street and off-street signs;
- abundance of utility poles and wires; and
- graffiti.

The litter problem is chronic and appears throughout the area. Littering includes stacked rubbish (discarded furniture, unwanted household items) as well as papers, bottles, and cans. In addition, vacant lots in the area are being used as dumping grounds.

STUDY AREA LAND USE--RESIDENTIAL

PLATE A-1



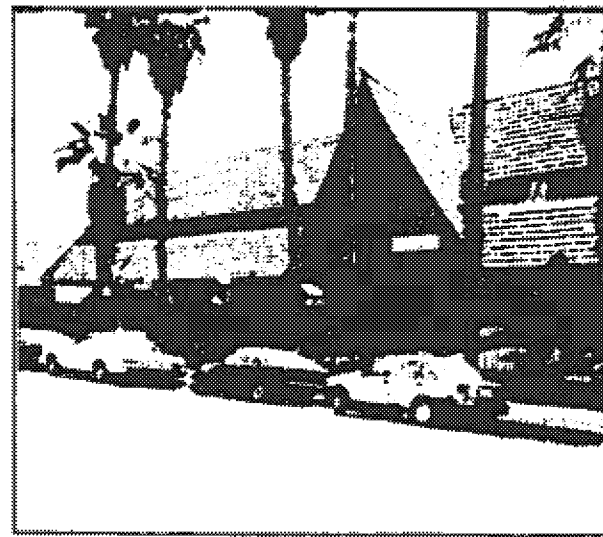
Typical Single Family House



Typical Single Family House



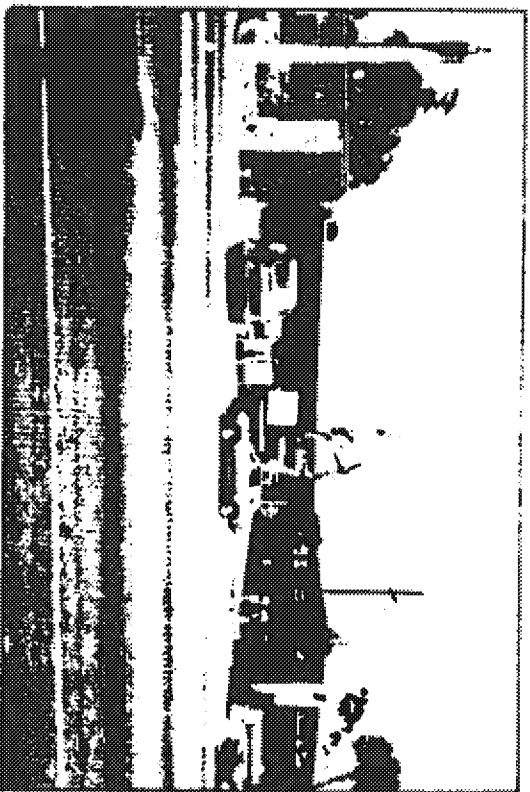
Typical Multi-Family, "Duplex Style" Residential Units



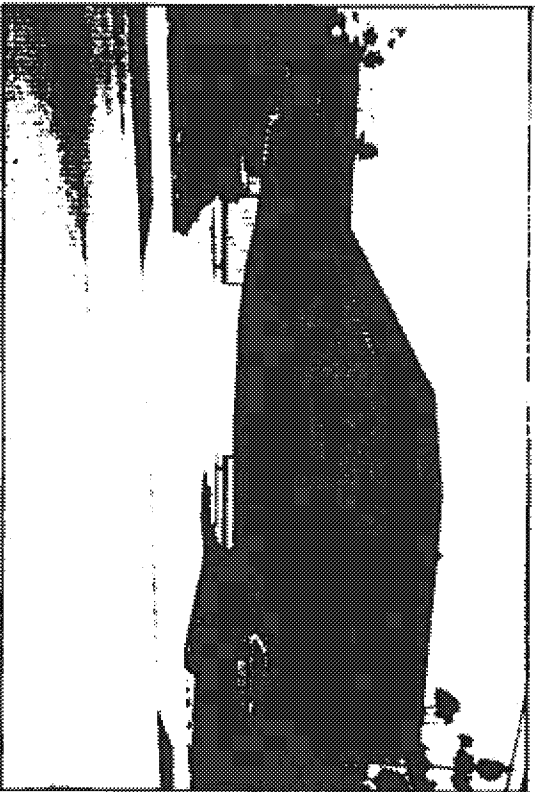
Apartment Complex

STUDY AREA LAND USE

PLATE A-2



Typical Commercial Structures



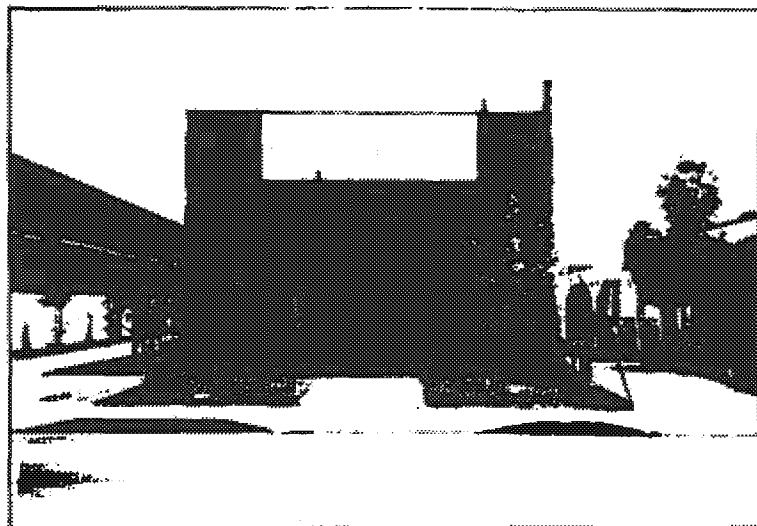
Typical Industrial Structures



Example of Institutional Land Use

STUDY AREA LAND USE MIX

PLATE A-3



Apartment Complex--Residential/Industrial/Multi-Family--Residential



Apartment Complex/Single Family--Residential



Multi-Family--Residential/Commercial

Century Boulevard

Prairie Avenue

Doty Avenue

Yukon Avenue

102nd Street

104th Street

106th Street

**EXISTING STRUCTURES
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**



GENERALIZED BUILDING FOOTPRINTS

INDUSTRIAL LOTS

FIGURE A-2



0 50 100 150 200 250 Ft.

Acres	
0.5	
0.25	

ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates

The lack of landscaping and poorly maintained yards adds to the visual blight. Very few yards are well maintained. The majority of the front yards which have grass and other landscape materials have degenerated to sparse and brown lawn with large weeds. Furthermore, some occupants park automobiles and other machinery in the front yards.

While infrequent, grass parkways are found on Prairie Avenue, Doty Avenue and 102nd Street (between Doty Avenue and Yukon Avenue). Yukon Avenue, 102nd Street (between Prairie Avenue and Doty Avenue), and 104th Street have sidewalks immediately adjacent the street. The grass parkways are not well maintained with much of them being dirt. Further, street trees are infrequent with the majority of them scattered along Doty Avenue, 102nd Street, and 104th Street. The street trees are so few that the signs and utility poles and wires have a larger effect on the visual quality than the trees.

Throughout the area, the streetscape is utilitarian; there are no special design features concerning public improvements. Public improvements are limited to streets and sidewalks. The sidewalks and streets are consistent width and run throughout the area; utility wires are all above ground as shown in Figure A-3. The utility poles and wires dominate the areas' visual character with the absence of foliage to obscure or hide them.

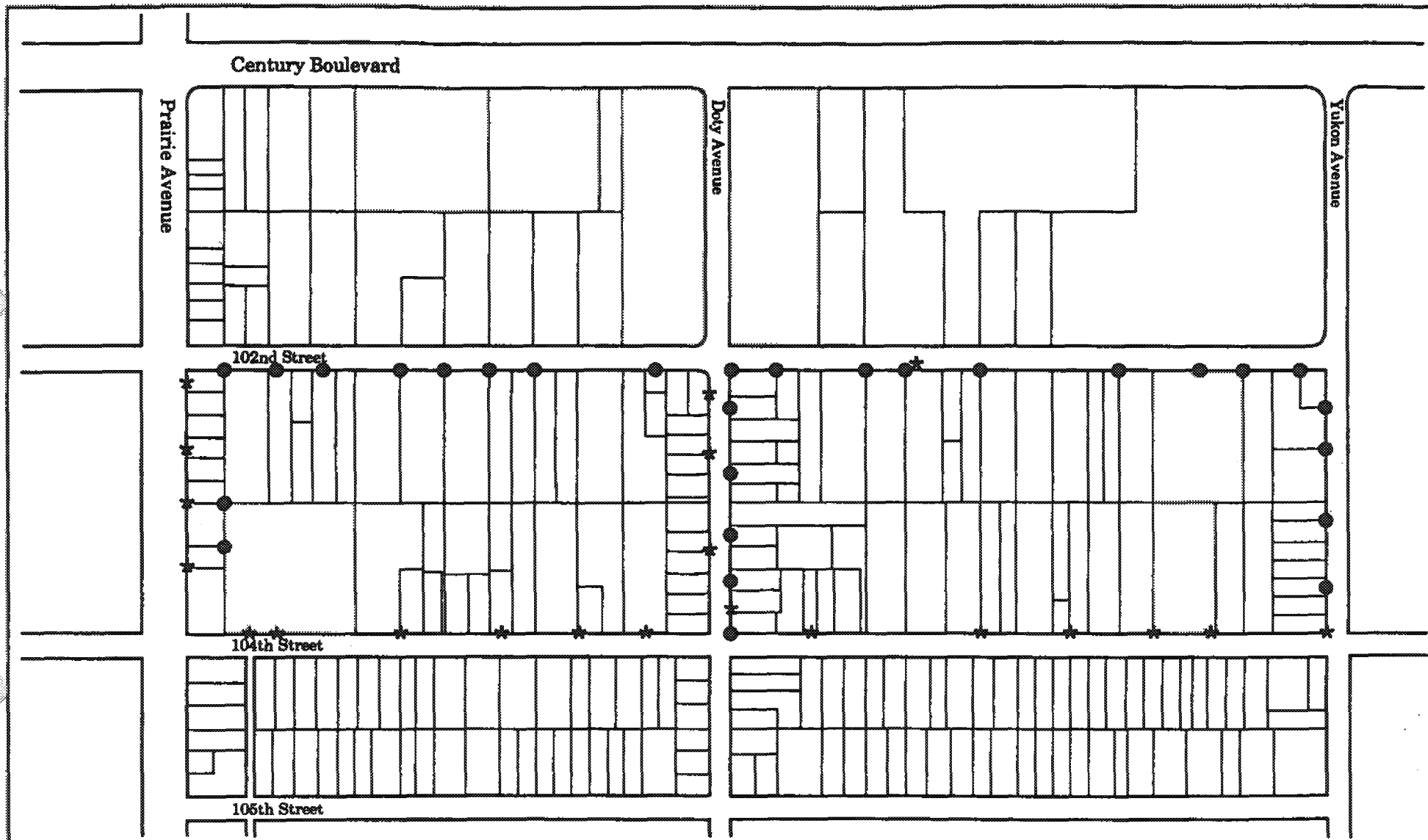
Signs in the area include small scale lighted panels, large arrows, and billboards. Three of the billboard signs are located on lots along Prairie Avenue, two of the three lots also contain a structure beneath the billboard. The billboards dominate the structures below. Other signs in the area are traffic related such as speed limit, school crossing, and stop signs. Signs throughout the area are so abundant that they become more confusing than helpful. In addition, the signs have become graffiti targets.

Graffiti is common throughout the area. Signs, buildings, lamp posts, curbs and trees are marked with graffiti.

In summary the urban design of the area can be summarized as disharmonious, harsh, poorly maintained, and utilitarian. Residential buildings do not have continuity. In effect, their scale and density create a visual hodgepodge. The industrial buildings are isolated. Their parking areas serve only one site while the property lines' fencing prohibits access to surrounding uses. Lastly, the streetscape lacks aesthetic amenities; pedestrians do not and would not venture between activities.

HOUSING

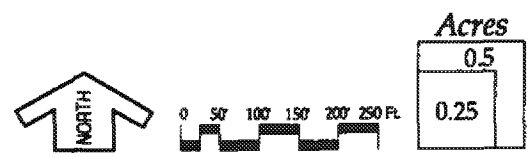
As discussed above, there are a total of 657 residential units scattered throughout the area. Of the 657 units, 42 are single family, 145 are in a "duplex" style, and 470 are located in apartment structures. The number of units in the area represent 1.7



**ABOVE GROUND UTILITIES
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

- ELECTRICAL POWER / TELEPHONE POLES
- ★ LIGHT POLES

FIGURE A-3



percent of the total units in the City. Using the city average for persons per household (preliminary 1990 census data) of 2.99, there are an estimated 1,964 persons residing in the area.

The single family units are generally in fair condition; however, two units have boarded windows and are condemned. Litter removal, new paint, and landscaping would improve the units' appearance. The apartment structures are two to three stories in height, and are primarily located along 102nd Street and 104th. Complex sizes range from 14 to 62 units. The complexes appear to be near fully occupied and in fair structural condition.

CIRCULATION

A. Existing Roadways and Conditions

The primary arterials adjacent and in the area are Century Boulevard directly to the north, Crenshaw Boulevard 1/2 mile to the east, Imperial Highway 3/4 mile to the south, and Prairie Avenue serving as the western area boundary. Figure 2 depicts the location of these roadways.

In the IIBP's vicinity, Century Boulevard is a divided six-lane arterial which extends east-west providing access to the Los Angeles International Airport, the San Diego Freeway and the Harbor Freeway. Crenshaw Boulevard is a divided six-lane road which extends north-south providing arterial access from the harbor area to the Mid-City area. Imperial Highway, a four lane east-west arterial, provides access to the Harbor and San Diego freeways and the Los Angeles International Airport area.

Prairie Avenue is a six-lane divided arterial, approximately 100 feet wide in the area. The Prairie/Century intersection is signalized. Prairie Avenue is anticipated to have an interchange with the proposed Century Freeway; thus serving as a major local arterial link with a regional transportation facility.

The eastern area boundary is Yukon Avenue. In the IIBP area, Yukon Avenue is 52 feet wide, and is a four lane undivided collector road. Yukon Avenue serves as an access to both the Price Club and Morningside High School. The Yukon/104th intersection is signalized; all other Yukon Avenue intersections have stop signs.

The northern area boundary, 102nd Street, is a local road approximately 56 feet wide. 102nd Street forms a "T" intersection with Yukon Avenue, and crosses Prairie Avenue to continue westward. All 102nd Street intersections have stop signs.

The southern area boundary, 104th Street, is approximately 56 feet wide and is considered a local facility. One Hundred Fourth (104th) Street intersects both Yukon Avenue and Prairie Avenue. The Yukon/104th intersection is signalized, all other 104th Street intersections are stop sign controlled.

Doty Avenue, north-south local road, bisects the area. This two-lane street is approximately 50 feet wide, and has stop sign controlled intersections. Doty Avenue intersects Century Boulevard and Imperial Highway, however, it shifts to the east at the Doty/107th Street intersection.

On-street parking is permitted throughout the area with restrictions. The restrictions are:

- Prairie Avenue - Two hour parking is permitted from 8 a.m. to 4 p.m. Parking is not permitted between 3:30 a.m. and 8 a.m., and 4 p.m. and 6 p.m.;
- Doty Avenue - Parking is not permitted from 8 a.m. to 12 p.m. on Thursdays;
- Yukon Avenue - Parking is not permitted from 9 a.m. to 1 p.m. on Fridays;
- 102nd Street - Parking is not permitted from 8 a.m. to 12 p.m. on Thursdays; and
- 104th Street - Parking is not permitted from 8 a.m. to 12 p.m. on Thursdays.

Currently, the surrounding roads are not congested nor do they have capacity problems during business hours because the arterials are designed for major vehicle generating events at Hollywood Park and the Great Western Forum. Local traffic counts were conducted on May 30, 1991, June 3 and 10, 1991, at six intersections in order to determine the existing traffic volumes. Peak hour traffic counts shown on Figure A-4 indicate that current traffic volumes are well below the streets capacities.

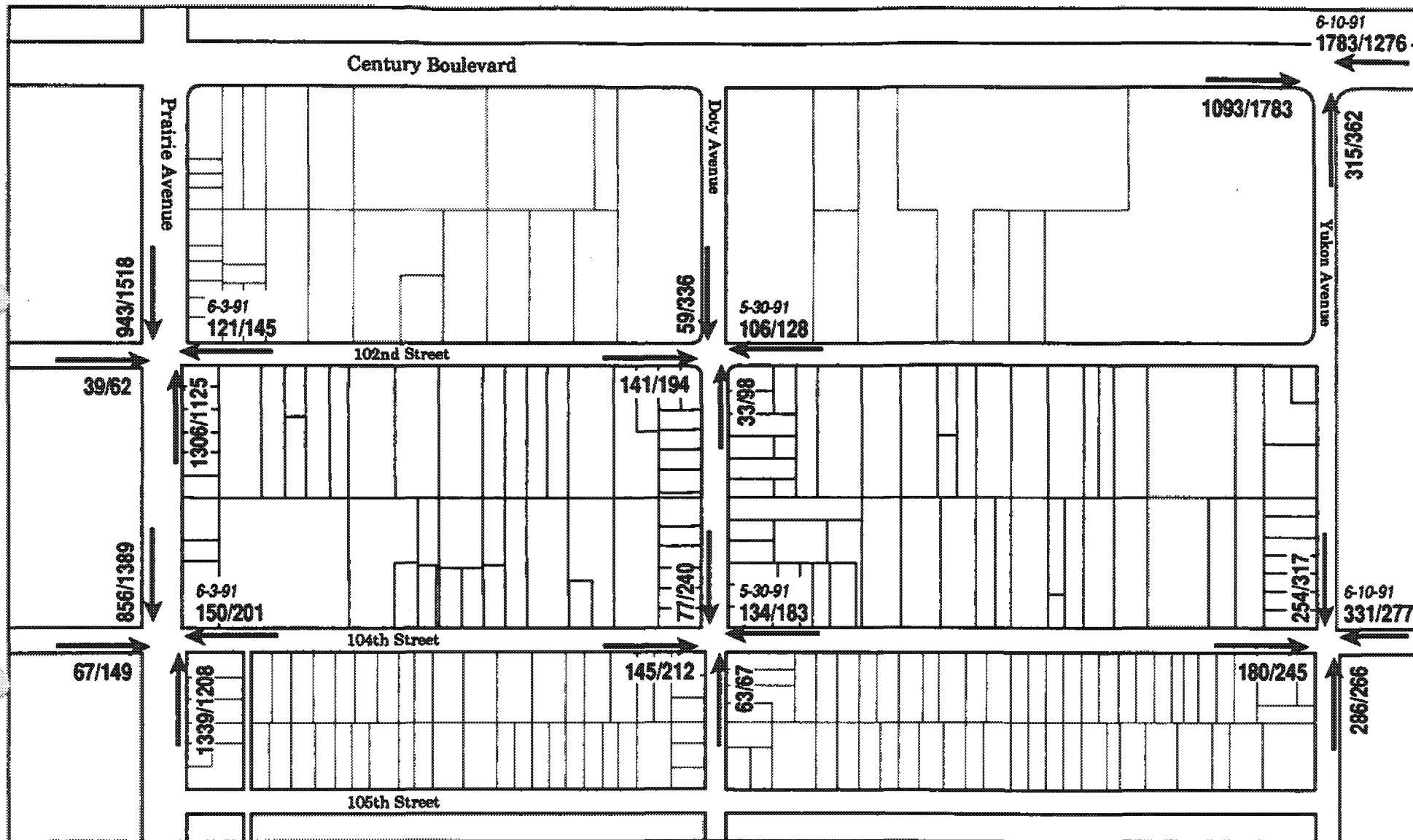
The areas' morning and evening peak hours are 7:45 to 8:45, and 4:45 to 5:45, respectively, with midday peaks at noon on Doty Avenue, and at 2:45 on Yukon Avenue. Both Doty Avenue and Yukon Avenue provide access to facilities which do not have "business hour" peaks (Hollywood Park, Price Club, and Morningside High School).

Truck routes provide a link between the interstate highway system and the local road system. Truck routes in the IIBP area and the immediate proximity are:

- Prairie Avenue from Imperial Highway to Florence Avenue;
- Crenshaw Boulevard from 118th Street to 80th Street;
- Century Boulevard from La Cienega Boulevard to Van Ness Avenue;
- 102nd Street from Prairie Avenue to Yukon Avenue; and
- Imperial Highway from Prairie Avenue to Van Ness Avenue.

B. Public Transportation

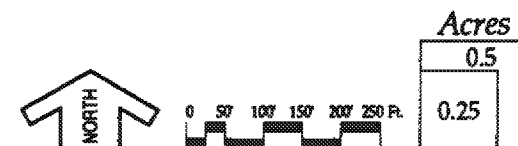
Southern California Rapid Transit District (SCRTD) services the IIBP area with two routes, Route 117 and Route 211. The Prairie/104th stop is the only bus stop located in the area as shown in Figure A-5. However, there are nine additional stops in proximity: Century/Yukon (two stops), Century/Doty (two stops), Century/Prairie (four stops), and the northwest corner of 104th/Prairie.

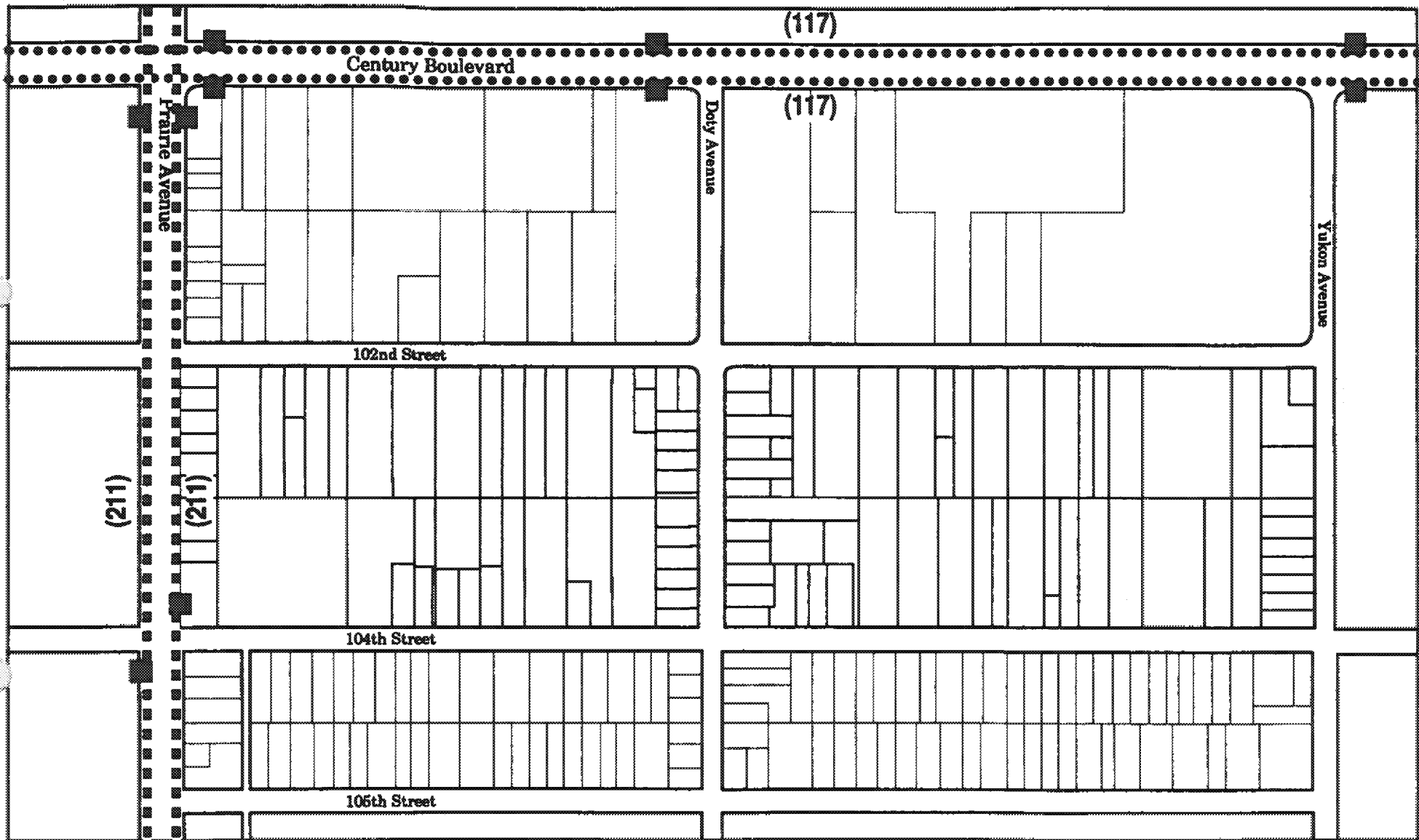


**A.M. / P.M. PEAK HOUR
TRAFFIC VOLUMES
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

A.M. — / — P.M.
6-3-91 Date(s) of Traffic Count

FIGURE A-4

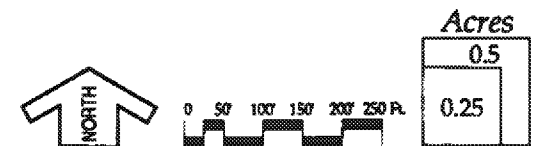




**PUBLIC TRANSPORTATION
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

- (211) ■ ■ ■ RTD ROUTES
(117)
■ BUS STOPS/BENCHES

FIGURE A-5



ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates

Route 117 operates on Century Boulevard between the City of Downey and the Los Angeles International Airport with a stop at the Blue Line Metro Station. Route 117 buses operate with a 15 minute headway, and run from 5 a.m. to 2 a.m. An average of 375 persons ride this route each day. At the Prairie/Century stops 343 persons board and 394 persons alight. At the Doty/Century stops, 236 persons board and 324 alight. In addition, at the Yukon/Century stops, 196 people board and 198 alight each day.

Route 211 operates on Prairie Avenue between the South Bay Galleria to Grace Avenue (north of Manchester Boulevard). The buses run every 30 minutes to 1 hour from 6 a.m. to 8 p.m. An average of 2,250 persons ride this route daily. At the two Prairie/Century bus stops, an average of 99 persons board and 104 alight the buses each day.

The number of persons alighting the buses in and near the area exceed the number of persons who board. The difference in ridership is most likely due to persons who ride the bus to Hollywood Park and use other modes to return.

C. Pedestrian and Bicycle Facilities

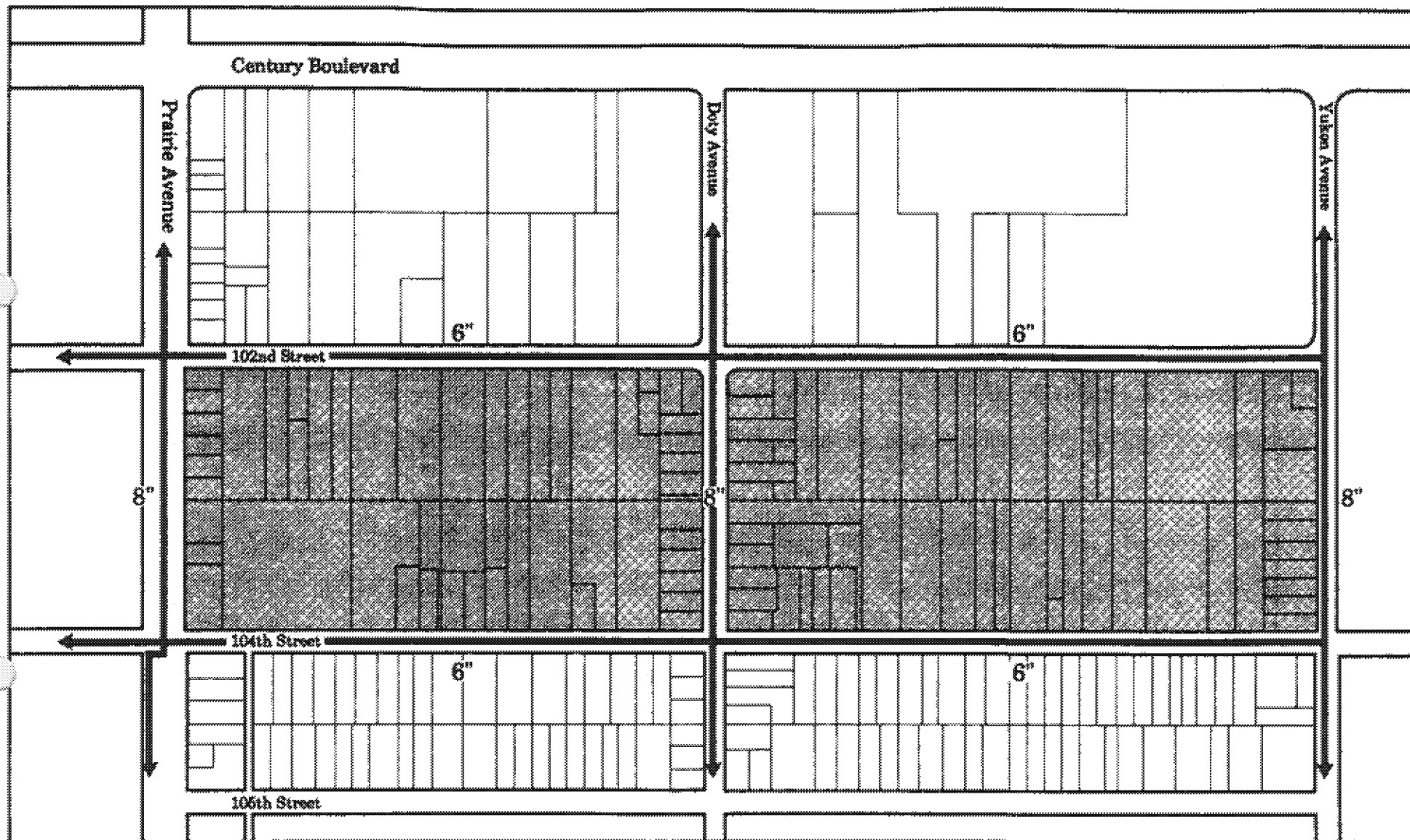
Sidewalks are located throughout the IIBP area. Sidewalk widths are approximately 4 feet in the area, although sidewalks on Prairie Avenue are approximately two feet wider than those on all other IIBP area streets. In general, the sidewalks are in fair condition, however, there are areas where the concrete has been cracked or is uneven due to nearby tree root growth.

Currently, 104th Street is designated as a Class III bike route. Class III bike routes are designated by a sign posted along the street and are not delineated by a painted stripe nor a separate biking facility.



UTILITIES

A. Water Service

The IIBP area is located in the Southern California Water Company (SCWC) service area. Eight-inch lines run beneath Prairie, Doty, and Yukon Avenues, as shown in Figure A-6. Beneath 102nd Street and 104th Street, six inch lines provide water service. According to SCWC, pressure and fire flows are sufficient to meet current demand, however, any significant increase in water usage or demand (e.g., changing from a low water using use to a higher water using use) may jeopardize the SCWC's ability to meet fire flow requirements. Fire flow test results taken on May 23, 1991 are:



WATER SYSTEM
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A

-  PROJECT SITE
- 6" — DIMENSION OF LINE (in inches)
-  TRANSITE (CONCRETE) PIPES



Acres	
0.5	
0.25	

FIGURE A-6

	Southeast corner of 102nd Street and <u>Doty Avenue</u>	West side of Doty Avenue 400 [±] feet south of <u>102nd Street</u>
Static Pressure	72	72
Residual Pressure	48	48
G.P.M. Flow Observed	919	2,148
G.P.M. Flow Calculated (at 20 lbs/sq. in.)	1,395	3,261

Due to the recent drought, SCWC is implementing two water use reduction programs: 1) mandatory 20 percent water rationing, and 2) use of reclaimed water. Reclaimed water is not currently available in the area, however, SCWC is planning to install reclaimed water lines by 1995.

SCWC does not currently plan to improve existing lines, although new water lines would be constructed if new developments strain existing facilities, if the streets are significantly improved, or if the existing lines have excessive maintenance problems.

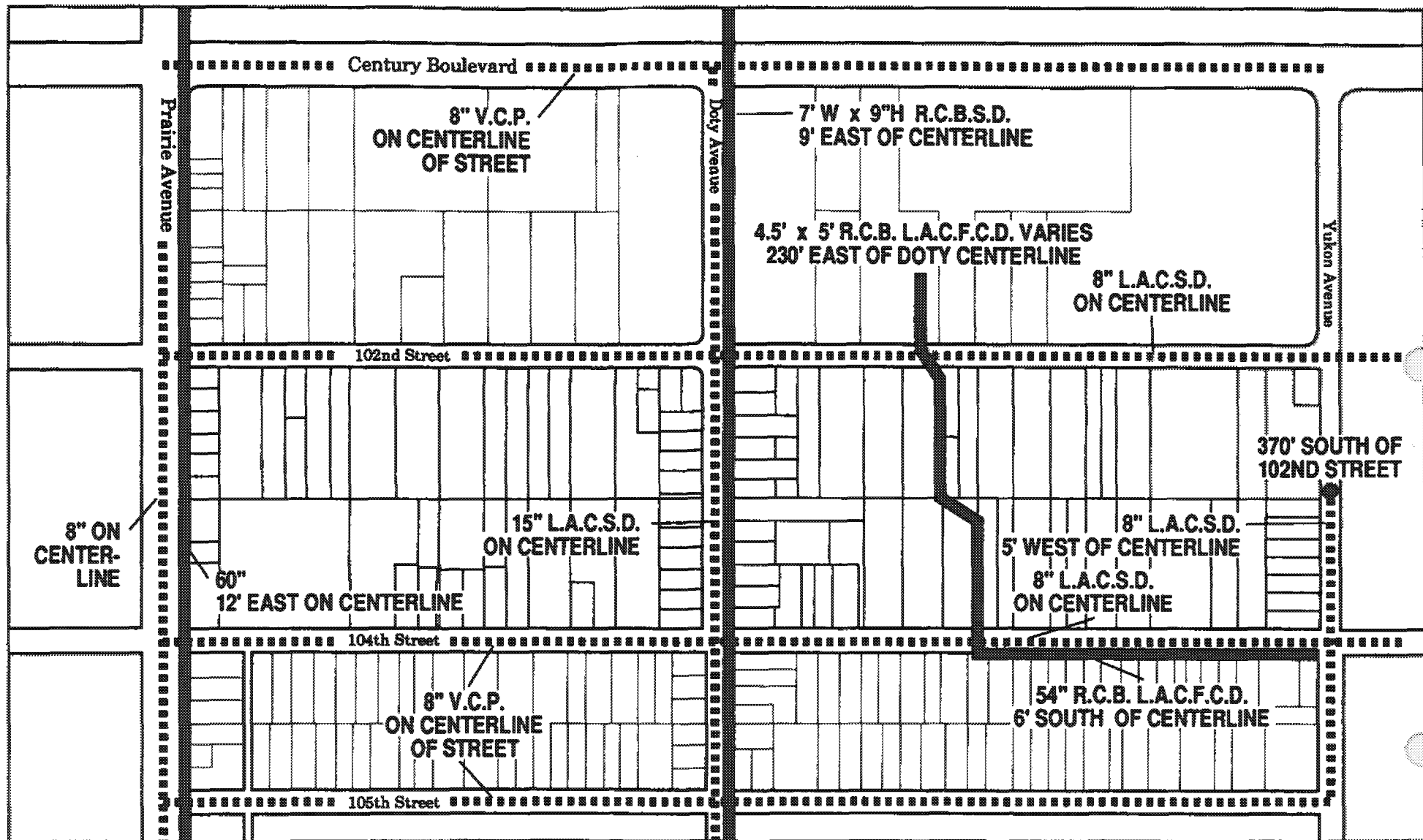
B. Storm Drains

The Los Angeles County Flood Control District (LACFCD) owns the mainline storm drain pipelines in the City. The City owns lateral lines and catch basins connecting with the LACFCD mainlines. The City's lateral lines and catch basins are maintained by the City. Maintenance of all storm drain facilities is conducted annually. No capital improvements are planned in the near future; although, any proposed connections with the mainline must be approved by the LACFCD prior to construction.

The storm drain water drains into the Dominguez storm channel and ultimately into the San Pedro Bay.

C. Sanitation Service

The City of Inglewood provides sewer service to the IIBP area. The entire area is sewered as shown on Figure A-7. Collector sewer lines are provided to each residence or building in the IIBP area and are located on Prairie Avenue and Yukon Avenue, 102nd Street and 104th Street. The local sewer lines connect with the Los Angeles County Sanitation District's 15-inch trunk line located within the Doty Avenue right-of-way. The County trunk line is currently operating at 40 percent of design capacity. Wastewater generated in the area is ultimately transported to the Joint Water Pollution Control Plant in Carson.



**STORM DRAINS /
SEWER SYSTEM**

**INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

— STORM DRAINS

..... SEWER LINES

FIGURE A-7



Acres
0.5
0.25

PUBLIC FACILITIES

A. Fire/Emergency Medical Service

The City of Inglewood maintains its own fire service. The City is divided into four districts, the IIBP area is in the South Region, District 4. The District's station is located at 107 South Crenshaw Boulevard approximately 3/4 mile from the project site. According to the Fire Department, emergency response time is approximately two minutes. The station is equipped with one pump truck, one paramedic truck, eight fire persons and two paramedics. The Fire Department does not anticipate any future additions or deletions to the current services. However, if and when the Lennox area is annexed, the South Region's jurisdiction would expand. This may require the relocation of the Crenshaw Boulevard station near Prairie Avenue.

B. Police Service

The City of Inglewood Police Department employs 210 police and administrative persons and 30 reserve officers. The police station is located at One Manchester Boulevard. The department has a total of 30 patrol cars and 15 unmarked cars. The IIBP area is located in one of 37 reporting districts, Reporting District 30. According to the Inglewood Police Department, District 30's emergency response time is three minutes. The Inglewood Police Department's 1990 Annual Report states District 30 had 132 incidents of crimes against persons (5.2 percent of all crimes against persons) and 200 incidents of crimes against property (3.3 percent of all crimes against property) in 1990.

ENVIRONMENTAL

A. Seismic

The following is excerpted and summarized from the City of Inglewood's Seismic Safety Element.

California's location makes it probable that the state will be affected by earthquakes; there is no known area in the state an individual can go to evade this probability.

Significant earthquakes which should be expected to occur in the foreseeable future and which should be considered in the design of structures in the City are of two distinct types: 1) major events generated by movement on a very large but relatively distant fault, and 2) medium-sized events generated by movement on a closer fault.

With regard to the first types, the most likely event is a Richter magnitude 8-8.5 earthquake on the San Andreas fault within the next 100 years. The shaking that would accompany this earthquake is expected to be only moderately strong in Inglewood because the source fault is 45 to 50 miles away. However, because of the length of the fault break and the way in which ruptures propagate, the shaking will probably last for at least one minute. For comparison purposes, the duration of the 1971 San Fernando earthquake was 12 to 15 seconds.

The Newport-Inglewood Fault extends through the City of Inglewood. This fault is parallel to the San Andreas system and lies partly under the Pacific Ocean. The trace on land starts near Newport Beach and extends northward along the Pacific coastline, past Signal Hill through Inglewood to a point somewhere near Culver City. A quake along this fault was responsible for extensive damage to Long Beach in 1933. However, most of the temblors felt in the Los Angeles area are diminished vibrations from more distant fault lines.

Several faults within the Newport-Inglewood zone of deformation traverse the City of Inglewood or are in proximity to the City. Four major faults trend northwest-southeast and are known from west to east as the Charnock, Inglewood, Townsite and Potrero Faults (Figure A-8). The latter three are cut or intersected by five northeast-southwest trending "transverse faults," all considered to be secondary faults to the major northwest-southeast trending faults (Fairview, Cemetery, Centinela, Manchester, and Century Faults). These faults are likely candidates for an intense earthquake (Table A-3).

The maximum probable earthquake are most likely to occur along the San Andreas Fault Zone (magnitude 8 at 45 miles) and the nearby Newport-Inglewood Zone (magnitude 6+ at less than 10 miles).

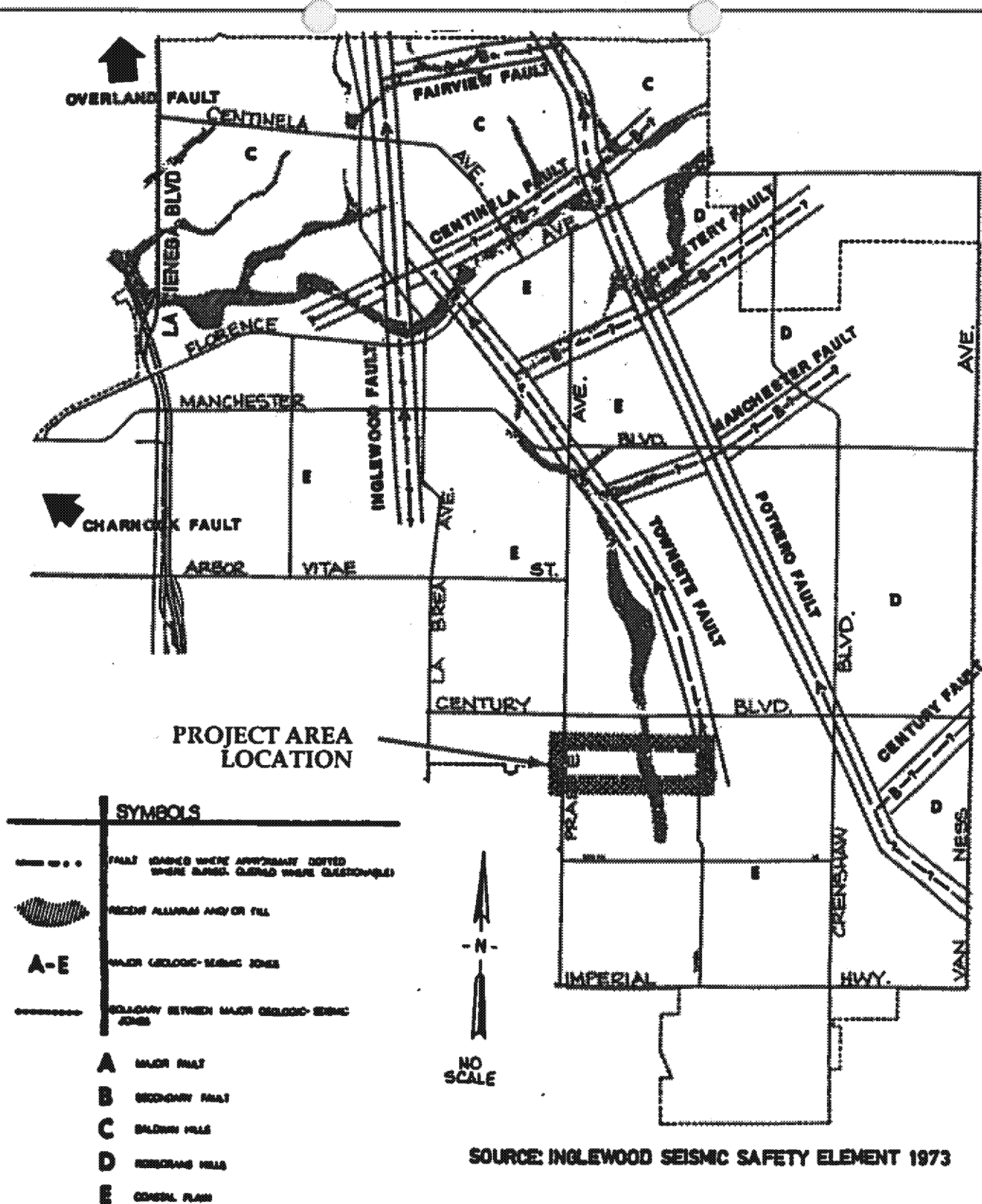
No faults are known to pass through the project site. However, the lack of on-site faulting does not eliminate potential damage which might result from a major earthquake on other faults in the region. The potential severity of ground shaking at the project site depends on many factors, including the distance from the originating fault, the earthquake magnitude and the nature of the earth materials beneath the site. The overall risk from seismic shaking in the Specific Plan area is the same as that for other sites in this general area of Inglewood.

B. Air Quality

The IIBP is located within the South Coast Air Basin, passing all of Orange County, most of Los Angeles and Riverside Counties, and the western portion of San Bernardino County.

Despite rigorous pollution control measures, the Basin still fails to meet the Federal Air Quality standards for four of the six criteria pollutants. Maximum ozone concentrations have reached three times the Federal standard, while carbon monoxide and fine particulate matter (PM₁₀) levels can be twice the Federal standard. In addition, the Basin is the only area in the country that fails to meet the nitrogen dioxide standard. The Federal standards for sulfur dioxide and lead are not exceeded in the Basin.

The air quality problem of the South Coast Air Basin comes from many sources. Motor vehicles are a major contributor, with a 40 percent increase in vehicle trips anticipated between 1985 and 2010 if no controls are implemented. Furthermore, the dispersed land use pattern common in southern California tends to separate jobs



EARTHQUAKE FAULT TRACES INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan-Appendix A

FIGURE A-8



ENVICOM CORPORATION in association with
 Economics Research Associates and Terry A. Hayes Associates

TABLE A-3**Local Faults Location**

Fault	Location	Approx. Distance From IIBP Site
Inglewood	West of La Brea from Hillcrest continuing north to Baldwin Hills (maximum credible earthquake, 7.0).	1 1/4 mile
Townsite	East of Yukon Avenue from 106th Street continuing northwest to Centinela and Inglewood Faults.	1/8 mile
Potrero	Van Ness at approximately 110th Street running northwest into and past Baldwin Hills (maximum credible earthquake 6.5).	1/2 mile
Fairview	North of Centinela between the Inglewood and the Potrero Faults.	2 3/4 mile
Centinela	North of Florence at Eucalyptus/Railreal running northeast past Potrero Fault and into Baldwin Hills.	2 1/4 mile
Cemetery	North of Manchester from the Townsite Fault heading northeast to Florence.	1 1/2 mile
Manchester	South of Manchester, east of Prairie (approximately the Great Western Forum), heading northeast past Crenshaw Boulevard.	1 1/8 mile
Century	South of Century Boulevard, approximately 106th Street running northeast past Van Ness.	3/4 mile

- Control of emission from both small and large boilers, generators and process heaters;
- Control of emissions from stationary gas turbines; and
- Waste recycling.

Mobile Sources

- Alternative work weeks and flextime;
- Telecommunications;
- Employer rideshare and transit incentives;
- Parking management;
- Vanpool purchase incentives; and
- High occupancy vehicle facilities.

C. Noise

The term "noise" is used to describe unwanted sound. The perception of the occurrence of noise is subjective as individual opinions vary as to what constitutes unwanted sound. Noise can generally be categorized into two types: background noise, which is a near-constant source of background sounds associated with a particular environment; and intrusive or peak noise, which are isolated events that stand out from background noise. Although both types of noise may affect the quality of life in a particular area, most environmental noise standards regulate background noise.

The primary concerns regarding community and environmental noise are the effects of noise on humans. Environmental noise is usually measured in A-weighted decibels (dBA), which is a frequency correction that correlates overall sound pressure levels with the frequency response of the human ear. In general, people can perceive a three-decibel change in noise level.

The three most common noise descriptors are Leq, Ldn, and CNEL. Leq is the actual time-averaged noise level, while Ldn and CNEL are 24-hour noise measurements that account for the greater sensitivity of most people to nighttime noise. Noise occurring between 10:00 p.m. and 7:00 a.m. is weighted 10 dBA higher than daytime noise. Ldn and CNEL, which are roughly equivalent, are most commonly used in establishing noise exposure guidelines for specific land uses.

Compatibility Guidelines for Noise Exposure

City of Inglewood Noise Ordinance

The City of Inglewood Noise Ordinance prohibits loud, unnecessary, and unusual noise which disturbs the peace and quiet of any neighborhood or which causes discomfort or annoyance to any reasonable person of normal sensitivity working or residing in the area. The ordinance establishes limits for residential and non-

residential noise by noise level and duration and prohibits construction noise between the hours of 8:00 p.m. and 7:00 a.m.

As part of the Noise Element of the General Plan, the City of Inglewood has established guidelines for noise and land use compatibility for new construction. The compatibility of community noise levels with various types of land uses are compared in the City's Noise/Land Use Matrix shown on Table A-5.

Federal Highway Administration Guidelines

The Federal Highway Administration has established guidelines for community noise based on the peak travel hour Leq (Table A-6). According to these guidelines, exterior peak hour Leq should not exceed 67 decibels for uses such as residences, churches, libraries, hospitals, picnic areas, playgrounds, and active sports areas.

Title 24, California Administrative Code

Title 24 specifies the maximum allowable sound transmission between dwelling units in new multi-family buildings and limits allowable interior noise levels in new multi-family residential units to 45 dBA CNEL. For new multi-family construction proposed in areas where the existing exterior noise level is greater than 60 dBA CNEL, Title 24 requires that an acoustical analysis be performed to demonstrate that interior noise levels would not exceed 45 dBA CNEL.

Noise Sources and Levels

The project site currently contains a mix of single and multi-family residential uses, warehousing facilities, and a church. The primary noise sources in the project vicinity are motor vehicle traffic on surrounding roadways and aircraft traffic from Los Angeles International Airport.

Sensitive Receptors

Some land uses are considered more sensitive to ambient noise levels than others, due to the amount of noise exposure and the types of activities typically involved. Residences, motels and hotels, schools, libraries, churches, hospitals, nursing homes, auditoriums, parks, and outdoor recreation areas are generally more sensitive to noise than are commercial and industrial land uses. Sensitive receptors in the vicinity of the project site include on-site single- and multi-family residences and the church and adjacent residential uses to the east, west and south.

Motor Vehicle Noise

Noise generated by motor vehicles increases with the volume and speed of traffic. Additionally, noise levels increase as the proportion of truck traffic increases. As the Level of Service on roadways decreases beyond about Level of Service C, reductions in traffic speeds offset increases in traffic volumes, and noise levels decrease.

TABLE A-5 - Noise/Land Use Compatibility Matrix

Land Use Categories		Community Noise Equivalent Level (CNEL)						
Categories	Uses	≤ 55	55-60	60-65	65-70	70-75	75-80	80+
Residential*	Single-family, duplex, multiple-family	A	A	B	B	C	D	D
Residential*	Mobile home	A	A	B	C	C	D	D
Commercial	Hotel, motel, transient lodging	A	A	B	B	C	C	D
Commercial Regional, Village District, Special	Commercial retail, bank Restaurant, movie theater	A	A	A	A	B	B	C
Commercial, Industrial, and Institutional	Office building, research and development, professional offices, City office building	A	A	A	B	B	C	D
Commercial Recreation Institutional Civic Center	Amphitheater, concert hall Auditorium, meeting hall	B	B	C	C	D	D	D
Commercial Recreation	Children's amusement park, miniature golf course, go-cart track, equestrian center, sports club	A	A	A	B	B	D	D
Commercial, Industrial, and Institutional	Automobile service station, auto dealership, manufacturing, warehousing, wholesale, utilities	A	A	A	A	B	B	B
Institutional	Hospital, church, library, schools' classroom	A	A	B	C	C	D	D
Open Space	Parks	A	A	A	B	C	D	D
Open Space	Golf course, cemeteries, nature centers, wildlife reserves, wildlife habitat	A	A	A	A	B	C	C
Agriculture	Agriculture	A	A	A	A	A	A	A

Interpretation

Zone A Clearly Compatible	Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction without any special noise insulation requirements.
Zone B Normally Compatible	New construction or development should be undertaken only after detailed analysis of the noise reduction requirements are made and needed noise insulation features in the design are determined. Conventional construction, with closed windows and fresh air supply systems or air conditioning, will normally suffice.
Zone C Normally Incompatible	New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of noise reduction requirements must be made and needed noise insulation features included in the design.
Zone D Clearly Incompatible	New construction or development should generally not be undertaken.

*Construction of new residential uses will not be allowed in the 65 CNEL for airport noise.

Source: Inglewood General Plan Noise Element, September, 1987.

TABLE A-6**Federal Highway Administration Peak Hour Noise Guidelines**

Activity Category	Noise Abatement Criteria Leq (dB(A))	Description of Activity Category
A	57 (Exterior)	Tracts of land where serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities essential if the area is to continue to serve its intended purpose. Such areas could include amphitheaters, particularly parks or portions of parks, open spaces, or historic districts which are recognized by appropriate local officials for activities requiring special qualities of serenity and quiet.
B	67 (Exterior)	Picnic areas, recreation areas, playgrounds, active sports areas, and parks which are not included in Category A and residences, motels, hotels, public meeting rooms, schools, churches, libraries, and hospitals.
C	72 (Exterior)	Developed lands, properties or activities not included in Categories A or B above.
D	-	For requirements on undeveloped lands, see paragraphs 11A and C of Federal Aid Highway Program manual, Volume 7, Chapter 7, Section 3.
E	52 (Interior)	Residences, motels, hotels, meeting rooms, schools, churches, libraries, hospitals, and auditoriums.

Source: U.S. Department of Transportation, Federal Highway Administration Federal Aid Program Manual, Volume 7, Chapter 7, Section 3, Procedures for Abatement of Highway Traffic Noise and Construction Noise, Washington, DC, May 14, 1976 (Revised version in Federal Register, Vol. 47, No. 131, P. 29653 Thursday, July 8, 1982).

The Highway Traffic Noise Prediction Model, developed by the Federal Highway Administration, was used to evaluate current noise conditions in the vicinity of the project site. This model accounts for traffic volumes, traffic speed, roadway geometry, and the proportion of truck traffic. The traffic volume assumptions used in this analysis were provided by the City of Inglewood. Heavy trucks were assumed to comprise 0-7 percent of all vehicular traffic in the project vicinity and average vehicle speeds were assumed to be between 25 and 35 miles per hour.

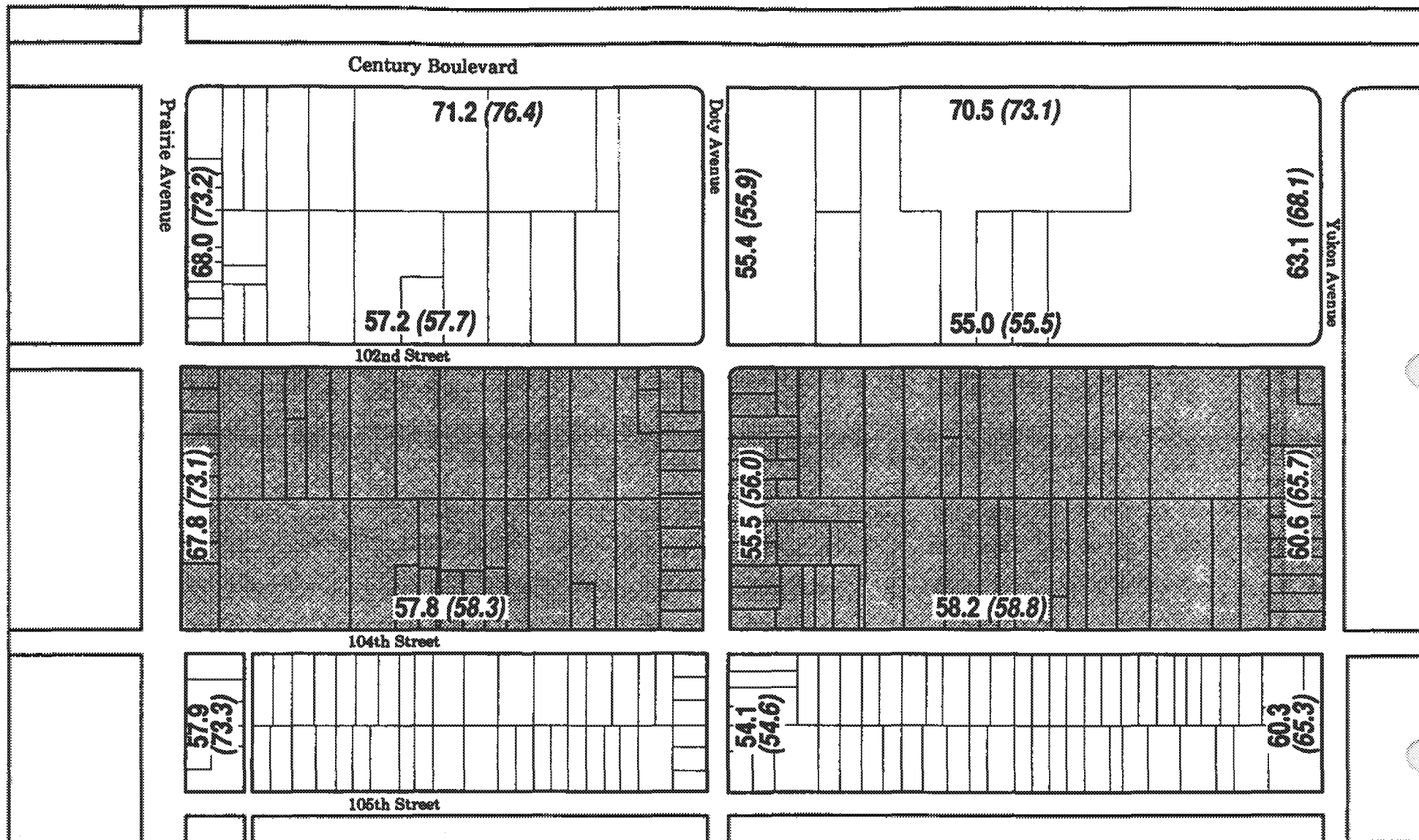
Table A-7 and Figure A-9 shows the peak hour Leq and CNEL values calculated at a distance of 50 feet from the centerline of each roadway segment assessed. Of the roadway segments immediately adjacent to the project site, only noise levels along Prairie between 102nd and 104th Streets exceed 65 dBA, based on traffic noise levels alone. However, the City of Inglewood Noise/Land Use Compatibility guidelines consider noise levels up to 70 dBA normally acceptable in areas of single- and multi-family residential uses, therefore, existing noise levels in the vicinity of the project site are considered compatible with existing land uses based on traffic noise alone. Because there are some motel and transient lodging facilities along the portion of Century Boulevard between Prairie Avenue and Doty Avenue, this segment, which has CNEL levels exceeding 70 dBA, is considered to be inconsistent with the City's Noise/Land Use Compatibility Guidelines. Federal Highway Administration peak hour noise standards are currently exceeded along Century Boulevard from Prairie Avenue to Yukon Avenue and along Prairie Avenue from Century Boulevard to 105th Street.

Aircraft Noise

Los Angeles International Airport (LAX) lies 1.5 miles west of the project site. The project site is beneath the takeoff and landing approach to the southernmost runways of LAX. Aircraft noise data in the form of a noise contour map, obtained from the Los Angeles Department of Airports, shows that the entire project site is within the 70 CNEL contour, as shown on Figure A-10. According to this map, single family, multi-family, and church uses are incompatible with areas where decibel levels exceed 65 CNEL. Therefore, the existing residential and church uses on the project site are incompatible with existing aircraft noise levels in the project vicinity. Table A-7 shows the existing noise levels in the project vicinity when traffic and airport noise levels are combined.

TABLE A-7**Existing Noise Levels**

Roadway Links	Traffic Peak Hour Leq	Traffic CNEL	Exceeds City Guidelines?	CNEL Including Airport Noise	Incompatible with Dept. of Airports Standards?	Exceeds FHA Standards?
Century Boulevard						
From Prairie to Doty	76.4	71.2	Yes	73.5	Yes	Yes
From Doty to Yukon	73.1	70.5	No	73.0	Yes	Yes
102nd Street						
From Prairie to Doty	57.7	57.2	No	70.2	Yes	No
From Doty to Yukon	55.5	55.09	No	70.1	Yes	No
104th Street						
From Prairie to Doty	58.4	57.8	No	70.3	Yes	No
From Doty to Yukon	58.5	58.2	No	70.3	Yes	No
Prairie Avenue						
From Century to 102nd	73.2	68.0	No	72.1	Yes	Yes
From 102nd to 104th	73.1	67.8	No	72.1	Yes	Yes
From 104th to 105th	73.3	57.9	No	70.3	Yes	Yes
Doty Avenue						
From Century to 102nd	55.9	55.4	No	70.1	Yes	No
From 102nd to 104th	56.0	55.5	No	70.2	Yes	No
From 104th to 105th	54.6	54.1	No	70.1	Yes	No
Yukon Avenue						
From Century to 102nd	68.1	63.1	No	70.8	Yes	No
From 102nd to 104th	65.7	60.6	No	70.5	Yes	No
From 104th to 105th	65.3	60.3	No	70.4	Yes	No

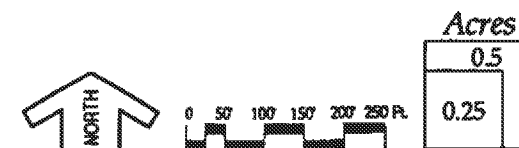


**EXISTING TRAFFIC
NOISE LEVELS
INGLEWOOD INTERNATIONAL
BUSINESS PARK
Specific Plan-Appendix A**

00.0 CNEL VALUE

(00.0) Leq VALUE

FIGURE A-9



B. Funding Program

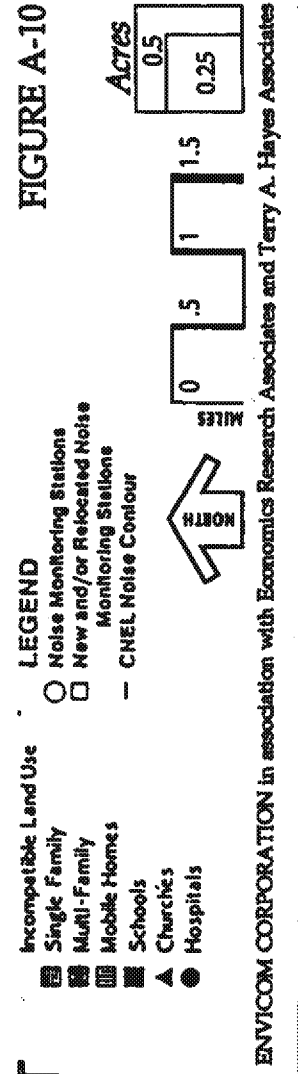
It shall be the policy and the implementation responsibility of the City of Inglewood:

1. To maximize investor, owner, and developer financial performance in the development of industrial properties in the Specific Plan Ordinance area.
2. To limit the City's financial participation in improvement of necessary traffic safety and circulation improvements in the public rights-of-way, and other such public improvements in public rights-of-way or in granted across private properties which are necessary for fire protection, storm water management, wastewater lines or other City of Inglewood Utility responsibility.
3. To limit private property purchases to those instances wherein public rights-of-way are required, or to circumstances of willing seller-willing buyer wherein unique opportunities for land assembly occur.
4. In any instance in which the City may purchase property, it shall be responsible for providing tenant relocation advisory services and payment of relocation benefits in the same manner and via the same eligibility and benefit schedules as may be in use by the City's Redevelopment Agency. The City shall require property owners whose property it purchases to carry out their own relocation and the city may provide relocation advisory services to such owners at its discretion.
5. The City of Inglewood is not obligate to by or to sell private properties in the Specific Plan Ordinance area except as may be required for public purposes of rights-of-way development. Any other private property purchase shall be at the City's sole discretion.
6. The City shall sell or lease any properties purchased for fair market value for feasible reuse as private industrial or commercial reuse consistent with the land use designations and allowable uses contained in the Specific Plan. The determination of fair market value shall be made according to the normal standards in the marketplace, including residual value analysis.
7. The City shall impose all normal and standard subdivision tract map and land division requirements upon the developers of industrial and commercial reuse properties in the Ordinance area, including appropriate requirements for public improvements necessitated by the private project.
8. The City may use, at it's sole discretion, eligible public funds for the implementation of the Specific Plan as may be necessary from time to time. Such public funds commitments for any purpose except relocation requirements shall not run for more than 10 years from the effective date of this ordinance unless expressly amended to extend said time limit.
9. Sources of funding for the implementation of the Specific Plan and for any public-private development agreements which are consistent with the purposes of the Ordinance and are in conformance with the development standards set out in this ordinance may include, but not be limited to, the following:

- City General Fund appropriations;
- Redevelopment Agency payback of City loans made to other Redevelopment Project Areas;
- Capital Improvement Program funds;
- Redevelopment Agency cost participation which is of direct benefit to the Century Redevelopment Project;
- FAA and City of Los Angeles Department of Airports airport noise abatement/use conversion funds;
- Other grant and/or loan funds as may be available from time to time;
- Proceeds from the sale of lease by the City of properties returned to private reuse within the ordinance area; and
- Loans and payments to the City from private developers which are a part of a public-private development agreement approved by the City for industrial or commercial reuse within the ordinance area.

L.A. INTERNATIONAL AIRPORT NOISE CONTOURS (CNEL) INGLEWOOD INTERNATIONAL BUSINESS PARK Specific Plan-Appendix A

FIGURE A-10



ENVICOM CORPORATION in association with Economics Research Associates and Terry A. Hayes Associates

C. Public Involvement

It shall be the policy and implementation responsibility of the City of Inglewood:

1. To hold a publicly noticed annual meeting for the owners, tenants, and residents of the ordinance area to describe progress and implementation prospects as well as provide direct public access to information, for a term not exceeding 10 years from the effective date of the Specific Plan Ordinance.
2. To publish from time to time, but not less often than once a year, a newsletter concerning the implementation progress of the Specific Plan Ordinance--to be mailed to each owner of record, business tenant, and residential tenant. A summary in Spanish shall also be provided.
3. All zone change, subdivision actions, and other matters requiring noticed public hearings regarding property development in the ordinance area shall be provided to the surrounding property owners as per the existing perimeter radius notification requirements of the City. The City shall also notice the property owners in the ordinance area.

D. Minority Business Enterprise Involvement

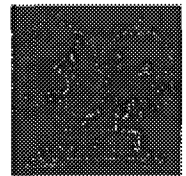
It shall be the policy and the implementation responsibility of the City of Inglewood:

1. To extend offers of first right to assemble properties for industrial development to the existing property owners, in the first year after the Specific Plan is adopted.
2. To provide existing business owners and business tenants with the opportunity to reestablish businesses within the Specific Plan Ordinance area, depending upon their capabilities and the industrial business type compatibility.
3. If the City or the Agency is involved in any property purchase--to extend an offer of relocation advisory service and limited relocation benefits to the selling owners and their tenants.
4. To offer Specific Plan Ordinance area property sellers (who sell to the City) opportunities to be developers, buyers, or tenants when such opportunities become available within the City's existing, ongoing redevelopment projects. Such opportunity shall be available for one year after City purchase of property in the Specific Plan area.
5. To establish Minority Business Enterprise (MBE) opportunity as one of the purposes of the Specific Plan and require City management to report upon results annually to the City Council over a 10-year period from date of ordinance adoption.
6. To provide guidelines to developers and property managers which encourage the provision of opportunities to MBE firms in the following areas:
 - As professional design, engineering, and development consultant firms on the developer team;
 - As development equity investors;
 - As tenants in developing projects; and
 - As ongoing business services provider (accounting, auditing, building and grounds maintenance, courier/delivery services, etc.).
7. To provide annual MBE opportunities matchup sessions for developers, for tenants, and for owners of Specific Plan area properties.
8. To provide notice of opportunities for business to the City's list of MBE establishments on a regular basis, as developments occur and as initiatives are marketed by the City in the Specific Plan area.

Responsibility for implementation of this section shall lie with the Office of the City Manager or such department as may be formally delegated by the City Manager.

Appendix B

Ownership Data



INGLEWOOD INTERNATIONAL
BUSINESS PARK

APPENDIX
INGLEWOOD BUSINESS PARK

	A	B	C	D	E	F	G
1	ASSESSING	STREET ADDRESS	SQUARE	EXISTING	BUILDING	GPLAN	EXISTING
2	PARCEL		FOOTAGE	LAND USE	DENSITY	DESIGNATION	ZONING
3							
4	4032-005-001	3660 102ND. ST.	24,631	MFR	3	INDUSTRIAL	M-1
5	4032-005-002	3650 102ND. ST.	10,947	SFR	1	INDUSTRIAL	M-1
6	4032-005-003	3646 102DN. ST.	10,947	SFR	1	INDUSTRIAL	M-1
7	4032-005-004	3640 102ND. ST.	21,894	MFR	6	INDUSTRIAL	M-1
8	4032-005-008	3612 102ND. ST.	18,853	APART.	21	INDUSTRIAL	M-1
9	4032-005-011	10301 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
10	4032-005-012	10305 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
11	4032-005-013	10309 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
12	4032-005-014	10313 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
13	4032-005-015	10317 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
14	4032-005-016	10323 YUKON AVE.	5,193	SFR	1	INDUSTRIAL	M-1
15	4032-005-017	10327 YUKON AVE.	5,193	MFR	1	INDUSTRIAL	M-1
16	4032-005-018	3615 104TH. ST.	18,245	APART.	21	INDUSTRIAL	M-1
17	4032-005-019	3621 104TH. ST.	18,853	VACANT	0	INDUSTRIAL	M-1
18	4032-005-023	3645 104TH. ST.	21,894	APART.	17	INDUSTRIAL	M-1
19	4032-005-024	3647 104TH. ST.	21,894	MFR	6	INDUSTRIAL	M-1
20	4032-005-025	3655 104TH. ST.	12,315	MFR	3	INDUSTRIAL	M-1
21	4032-005-035	10225 YUKON AVE.	15,208	MFR	7	INDUSTRIAL	M-1
22	4032-005-036	10221 YUKON AVE.	17,122	APART.	16	INDUSTRIAL	M-1
23	4032-005-037	3602 102ND. ST.	5,342	SFR	1	INDUSTRIAL	M-1
24	4032-005-038	3659 104TH. ST.	9,035	SFR	1	INDUSTRIAL	M-1
25	4032-005-039	3659 104TH. ST.	3,280	SFR	1	INDUSTRIAL	M-1
26	4032-005-040	3627 104TH. ST.	40,139	APART.	55	INDUSTRIAL	M-1
27	4032-005-041	3620 102ND. ST.	62,642	APART.	62	INDUSTRIAL	M-1
28	4032-006-002	10208 DOTY AVE.	4,250	SFR	1	INDUSTRIAL	M-1
29	4032-006-004	10224 DOTY AVE.	6,750	SFR	1	INDUSTRIAL	M-1
30	4032-006-005	10228 DOTY AVE.	6,750	MFR	4	INDUSTRIAL	M-1
31	4032-006-006	10218 DOTY AVE.	4,258	MFR	5	INDUSTRIAL	M-1
32	4032-006-007	3752 102ND. AVE.	50,505	SFR	1	INDUSTRIAL	M-1
33	4032-006-009		2,500	MFR		INDUSTRIAL	M-1
34	4032-006-010		2,150	MFR		INDUSTRIAL	M-1
35	4032-006-011	3746 102ND. ST.	15,204	MFR	3	INDUSTRIAL	M-1
36	4032-006-012	3734-38 1/2 102ND. ST.	30,409	MFR	8	INDUSTRIAL	M-1

INGLEWOOD BUSINESS PARK

	H	I
1	OWNER NAME	MAILING ADDRESS
2		
3		
4	KET CORPORATION	1420 W. 97TH. ST. L.A. 90047
5	KRAUSE, STEVE	9107 AVIATION BLVD. INGLEWOOD 90301
6	KRAUSE, STEVE	9107 AVIATION BLVD. INGLEWOOD 90301
7	SMITH, LLOYD L.	10220 CULVER BLVD. #201 CULVER CITY 90232
8	HOLTZMAN, LLOYD	P.O. BOX 1475 LA CANADA FLINTRIDGE 91011
9	COOPER, NATHANIEL	10301 YUKON AVE. INGLEWOOD 90303
10	SANCHEZ, JORGE	10305 YUKON AVE. INGLEWOOD 90303
11	KNOX, MELVIN	P.O. BOX 3870 BEVERLY HILLS, 90212
12	ONG, FRANCISCO	10313 YUKON AVE. INGLEWOOD 90303
13	BELL, HATTIE	10317 YUKON AVE. INGLEWOOD 90303
14	HENRY, LESLIE	10323 YUKON AVE. INGLEWOOD 90303
15	PLASCENCIA, JOSE	10327 YUKON AVE. INGLEWOOD 90303
16	KRIENER, JOEL	4921 BALTIMORE ST. L.A. 90047
17	LINDGREN, VIVIAN	4782 LA VILLA MARINA #C MARINA DEL REY 90292
18	WILD, CARYLL	10678 DRAKEWOOD CULVER CITY, 90230
19	BAY CITIES LOAN MORTGAGE	P.O. BOX 1497 TORRANCE 90505
20	BROOKENS, HATTIE	1505 W. 94TH. ST. L.A. 90047
21	PRESIDING BISHOP OF THE BAPTIST	4306 CRENSHAW BLVD. #200 L.A. 90008
22	TANG, RAYMOND	P.O. BOX 567 2050 SAN PEDRO COSTA RICA
23	IBBARRA, JOSE	3602 W. 102ND. ST. INGLEWOOD 90303
24	RAMIREZ, CARMEN	10525 RAVEUS WOOD AVE. LENNOX, 90304
25	RAMIREZ, CARMEN	3659 W. 104TH. ST. INGLEWOOD, 90304
26	KRIENER, JOEL	P.O. BOX 42792 L.A. 90042
27	GREEN, ROBERT	7 GEORGEFF ROLLING HILLS 90274
28	ADOLPHUS, CLARENCE	10208 DOTY AVE. INGLEWOOD 90304
29	ZAVALA, ANGEL	10224 DOTY AVE. INGLEWOOD 90303
30		
31	HUBBARD, WILLIE	3933 KENWAY AVE. L.A. 90008
32	TADEMY, R.L.	3752 W. 102ND. ST. INGLEWOOD 90303
33	HUBBARD, WILLIE	3933 KENWAY AVE. L.A. 90008
34		
35	CITIZEN, WALTER	610 W. 85TH. ST. L.A. 90044
36	LAULLILE, SUNIA	808 CALLE MARGARITA THOUSAND OAKS, 91360

INGLEWOOD BUSINESS PARK

	A	B	C	D	E	F	G
37	4032-006-013	3726 102ND. ST.	27,672	APART.	38	INDUSTRIAL	M-1
38	4032-006-014	3716 102ND. ST.	13,684	INDUSTRIAL		INDUSTRIAL	M-1
39	4032-006-015	3716 102ND. ST.	12,771	INDUSTRIAL	1	INDUSTRIAL	M-1
40	4032-006-016	3712 102ND. ST.	5,473	SFR	1	INDUSTRIAL	M-1
41	4032-006-017	3714 102ND. ST.	5,473	SFR	1	INDUSTRIAL	M-1
42	4032-006-018	3708-10 102ND. ST.	13,684	MFR	2	INDUSTRIAL	M-1
43	4032-006-019	3706 102ND. ST.	24,631	MFR	5	INDUSTRIAL	M-1
44	4032-006-020	3668 102ND. ST.	24,631	MFR	1	INDUSTRIAL	M-1
45	4032-006-021	3663 104TH. ST.	24,631	SFR	0	INDUSTRIAL	M-1
46	4032-006-022	3701 104TH. ST.	12,315	MFR	5	INDUSTRIAL	M-1
47	4032-006-023	3705 104TH. ST.	12,315	MFR	5	INDUSTRIAL	M-1
48	4032-006-024	3709 104TH. ST.	24,631	MFR	16	INDUSTRIAL	M-1
49	4032-006-025	3717 104TH. ST.	27,672	SCHOOL		INDUSTRIAL	M-1
50	4032-006-026	3729 104TH. ST.	27,672	VACANT	0	INDUSTRIAL	M-1
51	4032-006-027	104TH. ST.	7,500	VACANT		INDUSTRIAL	M-1
52	4032-006-028	3737 104TH. ST.	9,245	INDUSTRIAL	1	INDUSTRIAL	M-1
53	4032-006-031	3743 104TH. ST.	6,163	SFR	1	INDUSTRIAL	M-1
54	4032-006-032	3749 104TH. ST.	6,163	SFR	1	INDUSTRIAL	M-1
55	4032-006-033	3753 104TH. ST.	7,703	MFR	4	INDUSTRIAL	M-1
56	4032-006-035	10318 DOTY AVE.	5,500	SFR	1	INDUSTRIAL	M-1
57	4032-006-036	10314 DOTY AVE.	5,500	SFR	1	INDUSTRIAL	M-1
58	4032-006-037	10324 DOTY AVE.	5,409	SFR	1	INDUSTRIAL	M-1
59	4032-006-038	10310-10 1/2 DOTY AVE.	5,000	MFR	2	INDUSTRIAL	M-1
60	4032-006-039	10306 DOTY AVE.	5,000	MFR	2	INDUSTRIAL	M-1
61	4032-006-041	10212 DOTY AVE.	7,500	MFR	5	INDUSTRIAL	M-1
62	4032-006-042	10306 DOTY AVE.	10,000	MFR	4	INDUSTRIAL	M-1
63	4032-006-043	10302 DOTY AVE.	15,000	APART.	21	INDUSTRIAL	M-1
64	4032-006-044	10202 DOTY AVE.	6,100	SFR	1	INDUSTRIAL	M-1
65	4032-006-045					INDUSTRIAL	M-1
66	4032-006-046					INDUSTRIAL	M-1
67	4032-007-001	3850 102ND. ST.	30,409	APART.	40	INDUSTRIAL	M-1
68	4032-007-004	3820 102ND. ST.	30,409	INDUSTRIAL	1	INDUSTRIAL	M-1
69	4032-007-005	3818 102ND. ST.	22,909	APART.	28	INDUSTRIAL	M-1
70	4032-007-006	3812 102ND. ST.	5,000	SFR	1	INDUSTRIAL	M-1
71	4032-007-007	3806 102ND. ST.	5,000	SFR	1	INDUSTRIAL	M-1
72	4032-007-009	10211 DOTY AVE.	4,500	MFR	0	INDUSTRIAL	M-1

INGLEWOOD BUSINESS PARK

	H	I
37	KREINER, JOEL	4921 BALTIMORE ST. L.A. 90042
38	YU, PUI	19 BRIDGEPORT MANHATTAN BEACH 90266
39	YU, PUI	19 BRIDGEPORT MANHATTAN BEACH 90266
40		
41		
42	NAVARRO, JOSE	3710 W. 102ND. ST. INGLEWOOD 90302
43	GOOSBY, ALBERT	3700 W. 102ND. ST. INGLEWOOD 90303
44	KET CORPORATION	1420 W. 97TH. ST. L.A. 90047
45	BENNETT, JAMES	10670 LA CIENEGA BLVD. #B INGLEWOOD 90304
46	PADILLA, JUAN	11009 S. DALEROSE LENNOX, 90304
47	GUNGLE, HELEN	17019 CASIMIR AVE. TORRANCE, 90504
48	JOSEPH, RAY ENTERPRISES INC.	1534 10TH. ST. #5 SANTA MONICA 90405
49	HENDRICK, TOMMIE	10508 LA CIENEGA BLVD. #B INGLEWOOD 90304
50	SOCIEDAD, JOSE	P.O. BOX 3065 INGLEWOOD 90304
51	SOCIEDAD, JOSE	P.O. BOX 3065 INGLEWOOD 90304
52	DUTRA, RICHARD	P.O. BOX 5596 SANTA MONICA 90405
53	NEWTON, EDWARD	3743 W. 104TH. ST. INGLEWOOD 90303
54	GUEVARA, JOSE	3749 W. 104TH. ST. INGLEWOOD 90301
55	PRECIADO, ALFREDO	4448 W. EL SEGUNDO BLVD. #176 HAWTHORNE, 90250
56	SOSA, RAMON	10318 DOTY AVE. INGLEWOOD 90303
57	GUERRA, CARMEN	10314 DOTY AVE. INGLEWOOD, 90303
58	CARVAJAL, JOSE	10324 DOTY AVE. INGLEWOOD, 90303
59	NATTRESS, DONALD	4672 IRONWOOD SEAL BEACH, 90740
60	KENNEDY, FREDRIC	3774 NORTH LAND DRIVE L.A. 90008
61	ANAKWENZE, NWACHUKWU	3459 WESTMOUNT AVE. L.A. 90043
62	KENNEDY, FREDRIC	3774 NORTHLAND DRIVE, L.A. 90008
63	EVANS, LOUIS	5410 SENFORD AVE. L.A. 90056
64	JONES, RAE	10202 DOTY AVE. INGLEWOOD 90303
65	HERRERA, FRANCISCO	3712 W. 102ND. ST. INGLEWOOD 90303
66	JIMENEZ, ELIAS	2222 16TH. ST. SANTA MONICA 90405
67	HUMBER, DEWEY	4908 S. CRENSHAW BLVD. L.A. 90043
68		
69	POLE, RONALD	9801 S. BROADWAY L.A. 90003
70	FOWLKES, RONALD	13228 RUTHELEN ST. GARDENA 90249
71	BAY CITIES LOAN MORTGAGE	P.O. BOX 1497 TORRANCE 90505
72	BENAKASAS, PROSPER	14717 HAWTHORNE BLVD. HAWTHORNE 90250

INGLEWOOD BUSINESS PARK

	A	B	C	D	E	F	G
73	4032-007-010	10215 DOTY AVE	4,500	SFR	1	INDUSTRIAL	M-1
74	4032-007-011	10221 DOTY AVE.	4,500	SFR	1	INDUSTRIAL	M-1
75	4032-007-012	10225 DOTY AVE.	5,659	MFR	5	INDUSTRIAL	M-1
76	4032-007-013		1,250		0	INDUSTRIAL	M-1
77	4032-007-014	10301 DOTY AVE.	6,509	SFR	1	INDUSTRIAL	M-1
78	4032-007-015	10307 DOTY AVE.	4,800	SFR	1	INDUSTRIAL	M-1
79	4032-007-016	10311 DOTY AVE.	4,800	SFR	1	INDUSTRIAL	M-1
80	4032-007-017	10317 DOTY AVE.	4,800	SFR	1	INDUSTRIAL	M-1
81	4032-007-018	10321 DOTY AVE.	4,800	SFR	1	INDUSTRIAL	M-1
82	4032-007-019	10327 DOTY AVE.	4,700	SFR	1	INDUSTRIAL	M-1
83	4032-007-020	3813 104TH. ST.	30,409	MFR	6	INDUSTRIAL	M-1
84	4032-007-021	3821/3825-27 1/2 104TH. ST.	24,859	MFR	5	INDUSTRIAL	M-1
85	4032-007-022	3823 104TH. ST.	5,550	SFR	1	INDUSTRIAL	M-1
86	4032-007-023	3835 104TH. ST.	30,409	APART.	24	INDUSTRIAL	M-1
87	4032-007-024	3843-45 1/2 104TH. ST.	15,204	MFR	4	INDUSTRIAL	M-1
88	4032-007-025	3849-49 1/2 104TH. ST.	7,704	MFR	3	INDUSTRIAL	M-1
89	4032-007-026	3845-47 1/2 104TH. ST.	7,500	SFR	2	INDUSTRIAL	M-1
90	4032-007-028	3851 104TH. ST.	6,977	SFR	1	INDUSTRIAL	M-1
91	4032-007-030	3855 104TH. ST.	6,977	MFR	3	INDUSTRIAL	M-1
92	4032-007-031	3812 102ND. ST.	2,500	SFR	1	INDUSTRIAL	M-1
93	4032-007-032	3836 102ND. ST.	15,204	APART.	22	INDUSTRIAL	M-1
94	4032-007-033	3832 102ND. ST.	15,204	VACANT	0	INDUSTRIAL	M-1
95	4032-007-034	3853 104TH. ST.	16,455	APART.	18	INDUSTRIAL	M-1
96	4032-007-035	3838 102ND. ST.	15,204	INDUSTRIAL	2	INDUSTRIAL	M-1
97	4032-007-036	3844 102ND. ST.	15,204	MFR	4	INDUSTRIAL	M-1
98	4032-007-037	3800 102ND. ST.	5,000	SFR	1	INDUSTRIAL	M-1
99	4032-007-038					INDUSTRIAL	M-1
100	4032-008-001	10200 PRAIRIE AVE	4,485	VACANT	3	INDUSTRIAL	M-1
101	4032-008-002	10204-08 PRAIRIE AVE.	4,154	MFR	3	INDUSTRIAL	M-1
102	4032-008-003	10212-14 PRAIRIE AVE.	4,150	COMMERCIAL	1	INDUSTRIAL	M-1
103	4032-008-004	10216 PRAIRIE AVE.	4,150	VACANT	1	INDUSTRIAL	M-1
104	4032-008-005	10220 PRAIRIE AVE.	4,150	SFR	1	INDUSTRIAL	M-1
105	4032-008-006	10226 PRAIRIE AVE.	4,150	SFR	1	INDUSTRIAL	M-1
106	4032-008-007	3940 102ND ST.	30,409	APART.	30	INDUSTRIAL	M-1
107	4032-008-010	3926 102ND ST.	15,204	APART.	14	INDUSTRIAL	M-1
108	4032-008-011	3920 102ND. ST.	15,204	MFR	5	INDUSTRIAL	M-1

INGLEWOOD BUSINESS PARK

	H	I
73	EDMAN, WARREN	10215 DOTY AVE. INGLEWOOD 90303
74	SANCHEZ, ANNA	10221 DOTY AVE INGLEWOOD 90303
75	NELSON, BRENDA	P.O. BOX 3231 PACOIMA 91331
76	NELSON, BRENDA	P.O. BOX 3231 PACOIMA 91331
77	ANDERSON, HERBERT	10301 DOTY AVE. INGLEWOOD 90303
78	JONES, FRANCES	10307 DOTY AVE. INGLEWOOD 90303
79	BLACKWELL, LAURA	10311 DOTY AVE. INGLEWOOD 90303
80	BRYANT, DEMPSEY	10317 DOTY AVE. INGLEWOOD 90303
81	HOSEA, ALFRED	10321 DOTY AVE. INGLEWOOD 90303
82	RODRIGUEZ, CARLOS	12700 HAWTHORNE BLVD. HAWTHORNE 90250
83	GARCIA, FRANCISCO	17046 ROSCOE BLVD. NORHTRIDGE 91325
84	YASUDA INVESTMENTS	P.O. BOX Y TORRANCE 90507
85	BOYCE, ROBERT	17775 DEVERUEX ROAD SAN DIEGO 92128
86	WILBUR, KAISA	1852 W. LOMITA BLVD. #206 LOMITA 90717
87	ALADRO, MANUEL	5024 PICKFORD WAY CULVER CITY 90230
88	CORRAL, MARTIN	3377 MCLAUGHLIN AVE. L.A. 90066
89	CORRAL, MARTIN	3377 MCLAUGHLIN AVE. L.A. 90066
90	3851 W. 104TH. ST. LIMITED	1111 S. ARROYO PARKWAY #426 PASADENA 91105
91	GOMEZ, ISIDRO	3855 W. 104TH. ST. INGLEWOOD 90303
92	MARTIN, LESLIE	3812 W. 102ND. ST. INGLEWOOD 90303
93	BRADLEY, ROBERT	23735 SANDHURST LANE, HARBOR CITY 90710
94	ROBIN, ROLLIN CO.	5315 YARMOUTH AVE. #105 ENCINO 91316
95	3851 W. 104TH. ST. LIMITED	1111 S. ARROYO PARKWAY #426 PASEDNA 91105
96	BHALLA, KRIISHAN	5959 W. ADAMS L.A. 90016
97	ARROYO, CESARIO	3844 W. 102ND. ST. INGLEWOOD 90303
98	TRAMMELL, CLINTON	3800 W. 102ND. ST. INGLEWOOD 90303
99	102ND. ST. AND INGLEWOOD PARTNERS	8911 SOPHIA AVE. SEPULVEDA 91343
100	VENICE HONGWANJI BUDDHIST TEMPLE	12371 BRADDOCK DR. CULVER CITY, 90260
101	BUNN, ELEHUE	1730 W. 125 ST. L.A. 90047
102	GONZALES, MAURILIO	P.O. BOX 4388 PACOIMA, 91331
103	UNION HOME LOANS	7840 MADISON AVE. #150 FAIR OAKS, 95628
104	FAGGOUSEH, ELIAS	15027 KINGDALE AVE. LAWDALE, 90260
105	COBO, DIEGO	P.O. BOX 2065 SOUTH GATE, 90280
106	REZEX, ABRAHAM J.	9550 LEMORAN DOWNEY, 90241
107	DUNN, GERALD D.	500 W. 64 PLACE INGLEWOOD, 90302
108	NELSON, ALBERT	1844 17TH ST. SANTA MONICA, 90404

INGLEWOOD BUSINESS PARK

	A	B	C	D	E	F	G
109	4032-008-012	3910 102ND. ST.	30,409	APART.	42	INDUSTRIAL	M-1
110	4032-008-013	3900-07 102ND. ST.	30,409	MFR	7	INDUSTRIAL	M-1
111	4032-008-014	3861 104TH. ST.	6,471	MFR	2	INDUSTRIAL	M-1
112	4032-008-015	3905 104TH ST.	7,622	MFR	2	INDUSTRIAL	M-1
113	4032-008-021	10318 PRAIRIE AVE.	12,789	VACANT		INDUSTRIAL	M-1
114	4032-008-022	10310 PRAIRIE AVE.	4,150	SFR	1	INDUSTRIAL	M-1
115	4032-008-025	3863 104TH ST.	6,300	MFR	6	INDUSTRIAL	M-1
116	4032-008-026	10300-08 1/2 PRAIRIE AVE.	8,300	COMMERCIAL	6 STORES	INDUSTRIAL	M-1
117	4032-008-029	3911 104TH ST.	30,409	APART.	44	INDUSTRIAL	M-1
118	4032-008-030	3930 102ND. ST.	6,000	SFR	1	INDUSTRIAL	M-1
119	4032-008-032	3947 104TH. ST.	129,238	SCHOOL/CHURCH	SCHOOL	INDUSTRIAL	M-1
120	4032-008-033	3936 102ND. ST.	15,204	APART.	1	INDUSTRIAL	M-1
121	4032-008-034		9,204	APART.		INDUSTRIAL	M-1

INGLEWOOD BUSINESS PARK

	H	I
109	PARTRIDGE, WILLIAM	340 S. RENO ST. #102 L.A. 90057
110	GAXIOLA, RONALD	P.O. BOX 85848 SAN DIEGO, 92138
111	HAYES, LUTHER	4930 INADALE AVE, L.A. 90013
112	GALVEZ, TELMA	3903 W. 104TH. ST. INGLEWOOD, 90303
113	MORI, ROY	2400 MANCHESTER BLVD. INGLEWOOD 90305
114	WADE, DONALD	1200 E. 220TH ST. CARSON, 90746
115	SANTANA, ISIDORO	3936 W. 112TH. ST. INGLEWOOD 90303
116	SANTORO, JULIO	11634 BOS ST. CERRITOS, 90703
117	HOLMES, THOM	6022 CONDON AVE, L.A. 90056
118	PRECIADO, CELESTINO	3930 W. 102ND. ST. INGLEWOOD 90303
119	PACIFIC CHRISTIAN COLLEGE ET	905 S. EUCLID AVE #105 FULLERTON, 92632
120	PRECIADO, CELESTINO	3936 W. 102ND. ST. INGLEWOOD 90303
121	WEST WORLD INVESTMENTS	11105 S. WESTERN AVE. L.A. 90047